



RICHMOND HIGHWAY
CORRIDOR IMPROVEMENTS
www.virginiadot.org/richmondhighway



HARMONY PLACE TRAILER PARK MEETING

THURSDAY, FEBRUARY 6, 2020
GOOD SHEPHERD CATHOLIC CHURCH
8710 MOUNT VERNON HIGHWAY
ALEXANDRIA, VIRGINIA 22309

Dan Reinhard, PE, VDOT - Design Project Manager

Ajmal "AJ" Hamidi, PE, FCDOT - Transportation Planner

Vanessa Aguayo, PE, FCDOT – BRT Project Manager

John McDowell, PE, RK&K – Project Manager

Lorraine Barksdale, PE, RK&K – Roadway Engineer

UPC 107187 Richmond Highway Corridor Improvements



Today's Meeting

- **Project Coordination and Overview**
- **Roadway Improvements**
- **Pedestrian Underpasses**
- **Stormwater Management**
- **Environmental Assessment**
- **Noise Analysis**
- **Harmony Place Trailer Park**
- **Right-of-Way Acquisition**
- **Schedule and Funding**
- **GIS Web Application**
- **Comments and Questions**

Project Coordination



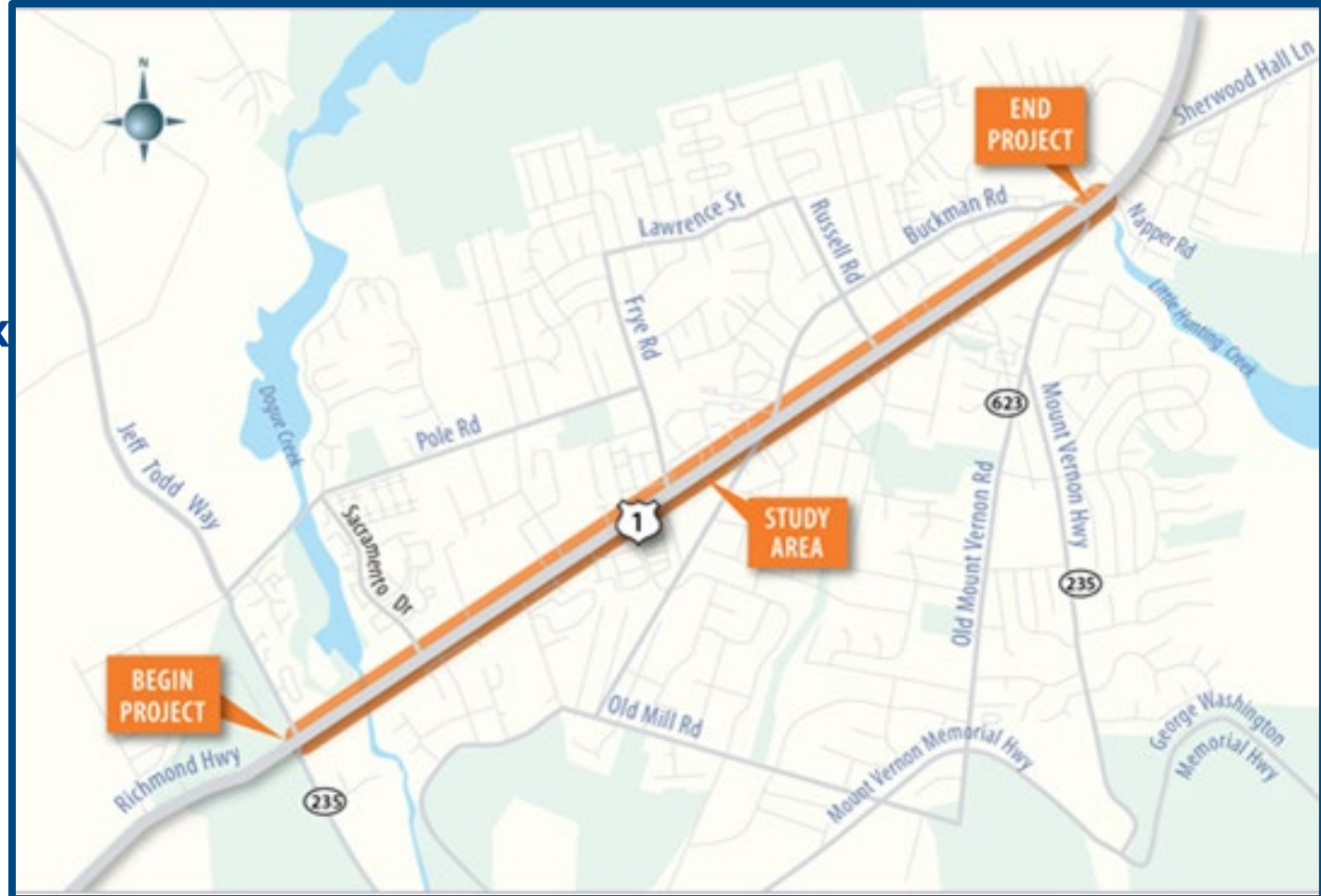
Embark Richmond Highway is an initiative focused on creating a multimodal future for Richmond Highway Corridor where residents, workers, and visitors can walk, bike, or drive to the places they want to go. The components of the Embark Richmond Highway project include:



- ⇒ Fairfax County-led components of EMBARK are providing the vision for the design of the widening of Richmond Highway from Jeff Todd Way to Sherwood Hall Lane.
- ⇒ Fairfax County Board of Supervisors approved the Embark Comprehensive Plan Amendment on March 20, 2018
- ⇒ Coordination between state and local agencies occurs regularly at project progress meetings and during informal discussions and mini-workshops.
- ⇒ VDOT maintains Richmond Highway and makes final decisions considering the positions of its partners and the public
- ⇒ Fairfax County Board of Supervisors will be asked to provide endorsement of this project.

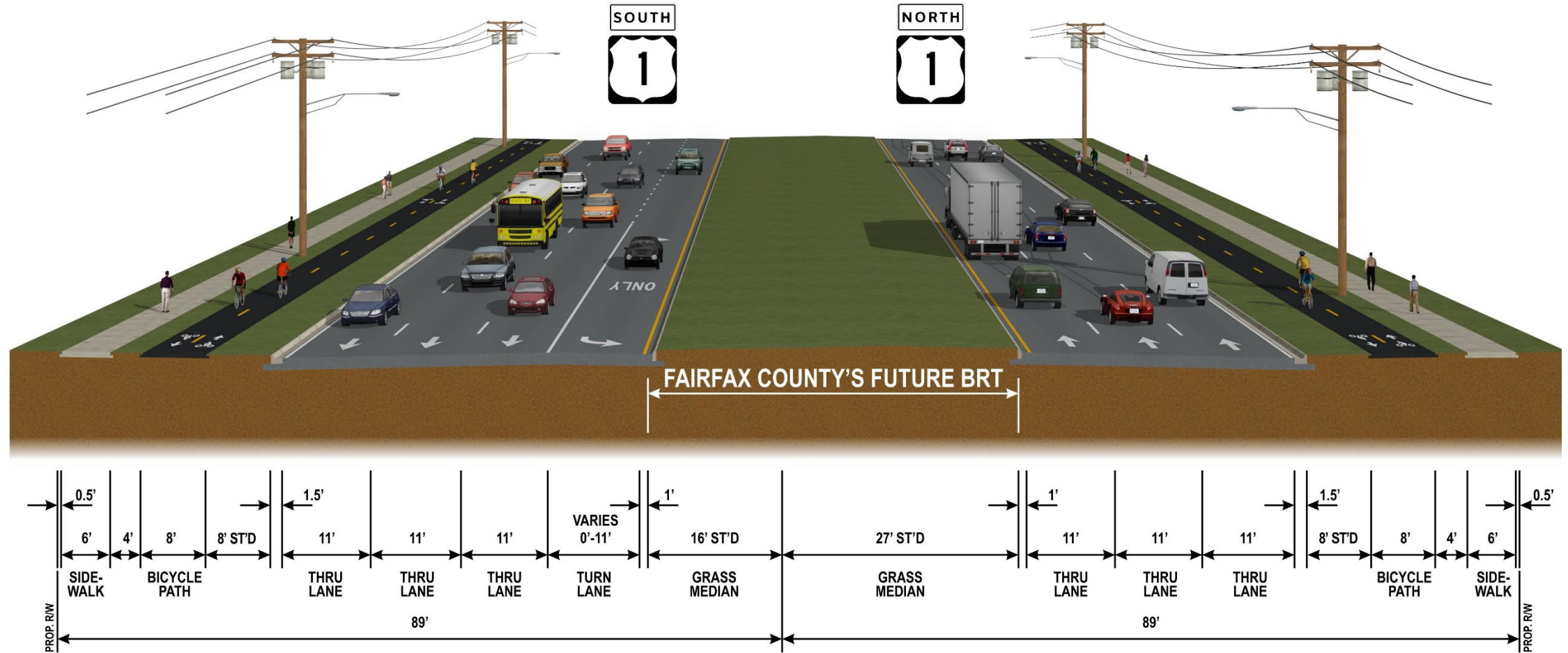
Project Overview

- **Widening of Richmond Highway (US Route 1) from four lanes to six lanes**
 - Jeff Todd Way to Sherwood Hall Lane
 - Approximately 3 miles
- **Safety improvements**
- **Congestion relief**
- **Intersection improvements**
- **Median reservation for future Fairfax County Bus Rapid Transit (BRT)**
- **Sidewalks and separate bicycle paths on both sides of road**
- **Three bridge replacements**
- **Potential noise walls**
- **Utility relocations**
- **Stormwater management**





Typical Cross Section

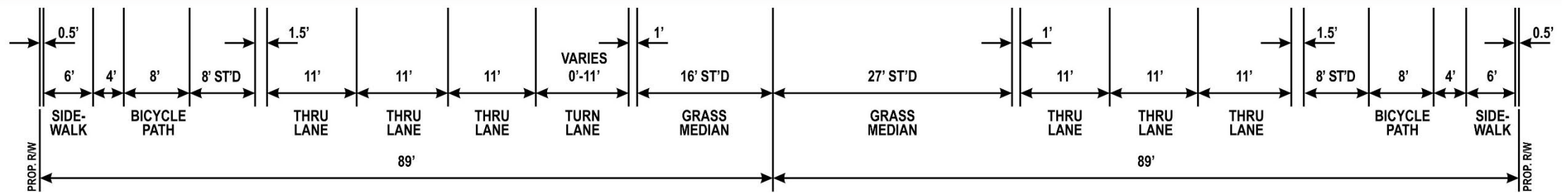
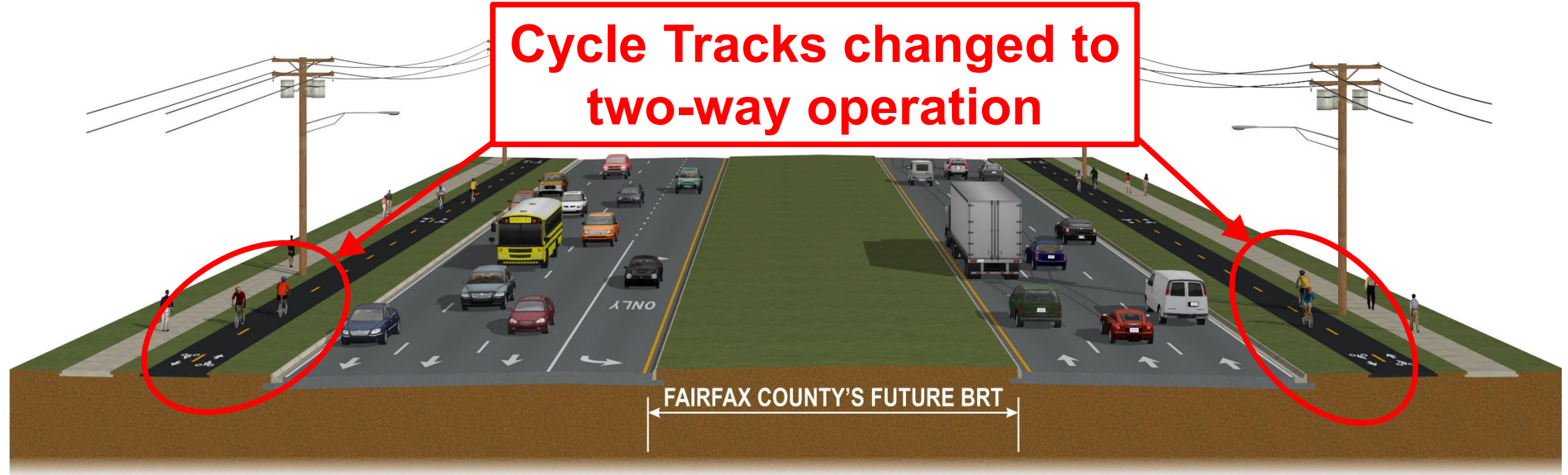


The intent of this exhibit is to depict the configuration of elements within the proposed right-of-way width of 178 ft. For clarity, potential landscaping is not depicted.



Typical Cross Section

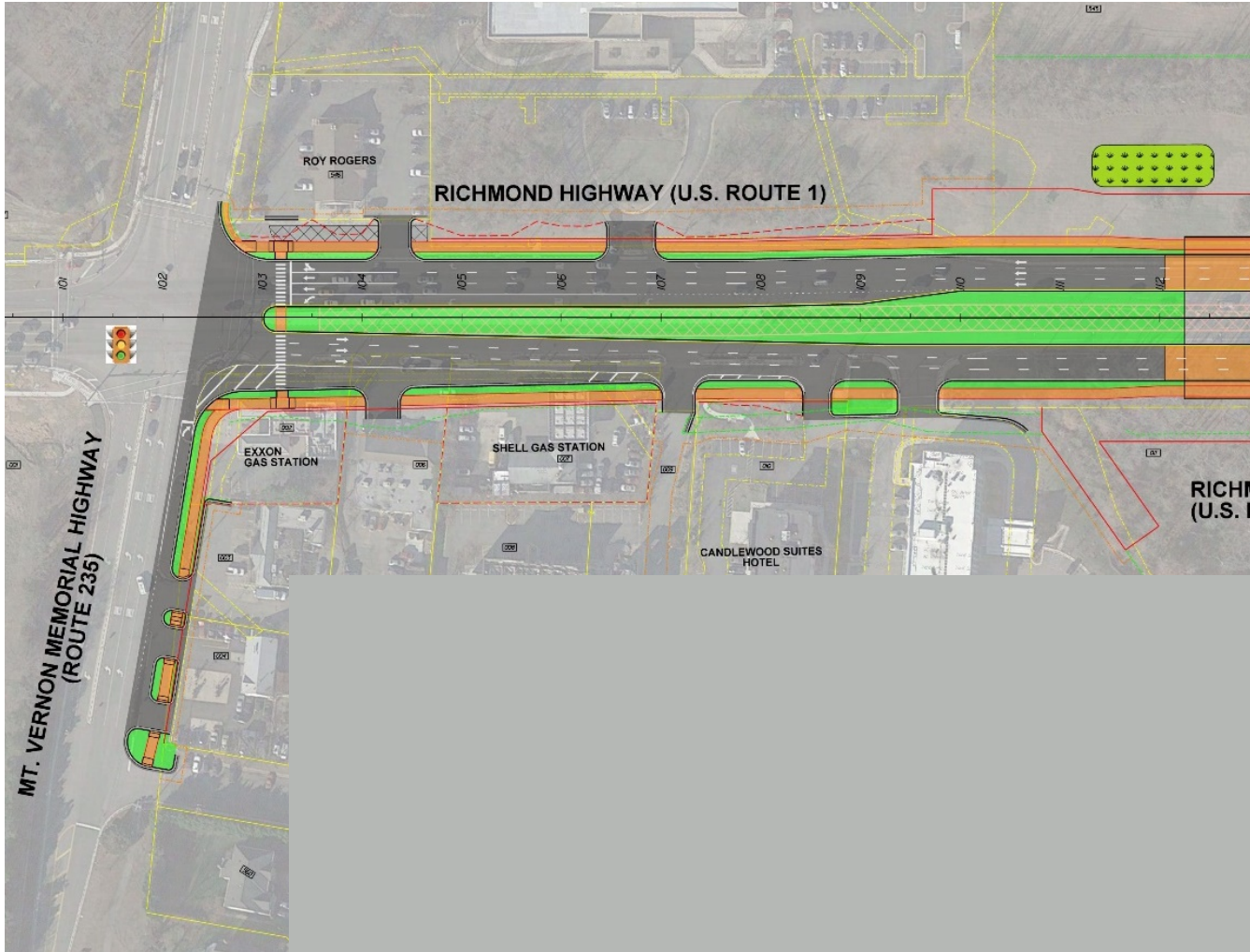
Cycle Tracks changed to two-way operation



The intent of this exhibit is to depict the configuration of elements within the proposed right-of-way width of 178 ft. For clarity, potential landscaping is not depicted.



Intersection Improvements



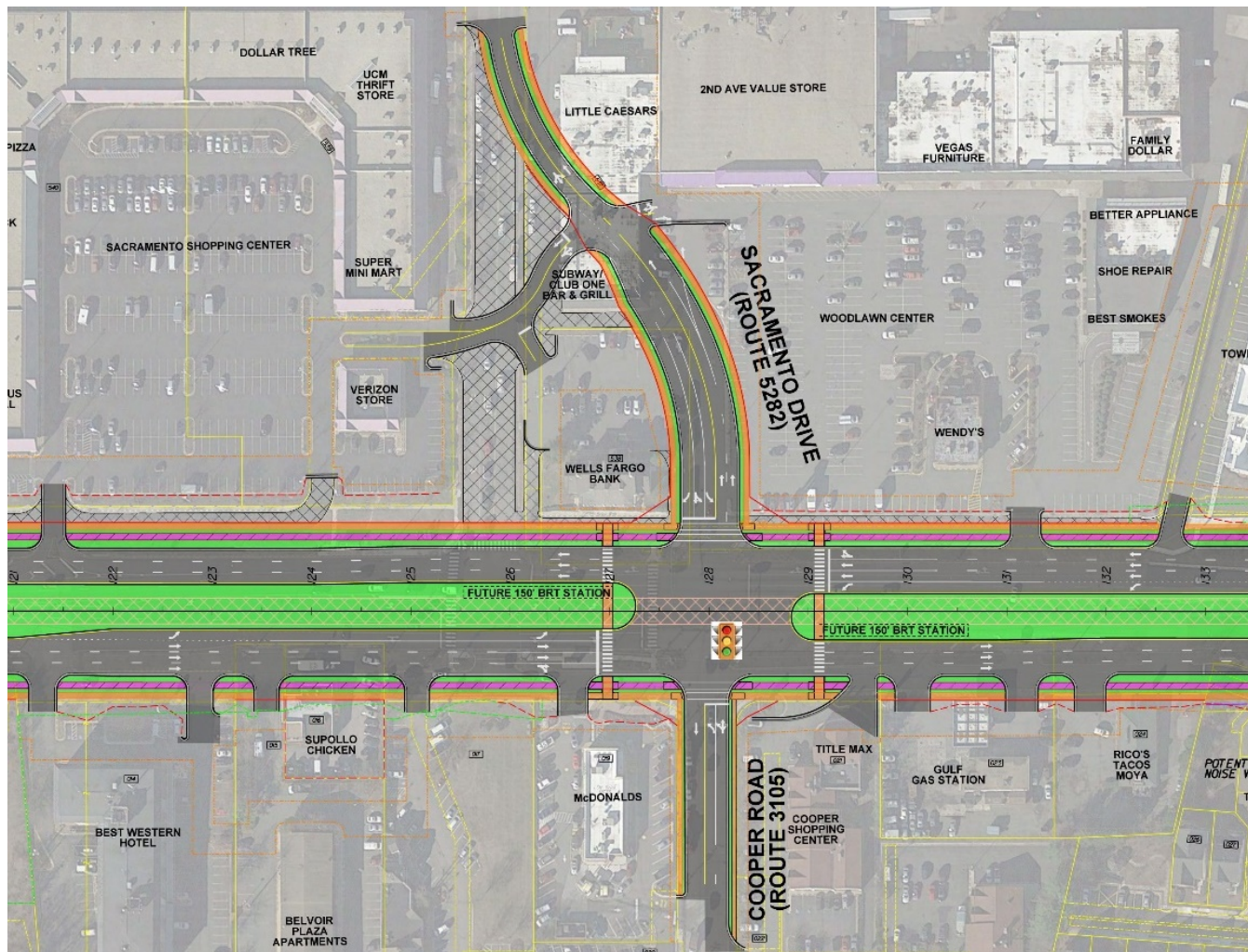
Mt. Vernon Memorial Highway / Jeff Todd Way

- Six Lanes along Richmond Highway
- Allows adjustments for future BRT accommodations
- Provides exclusive right-turn lane along Mt. Vernon Memorial Highway

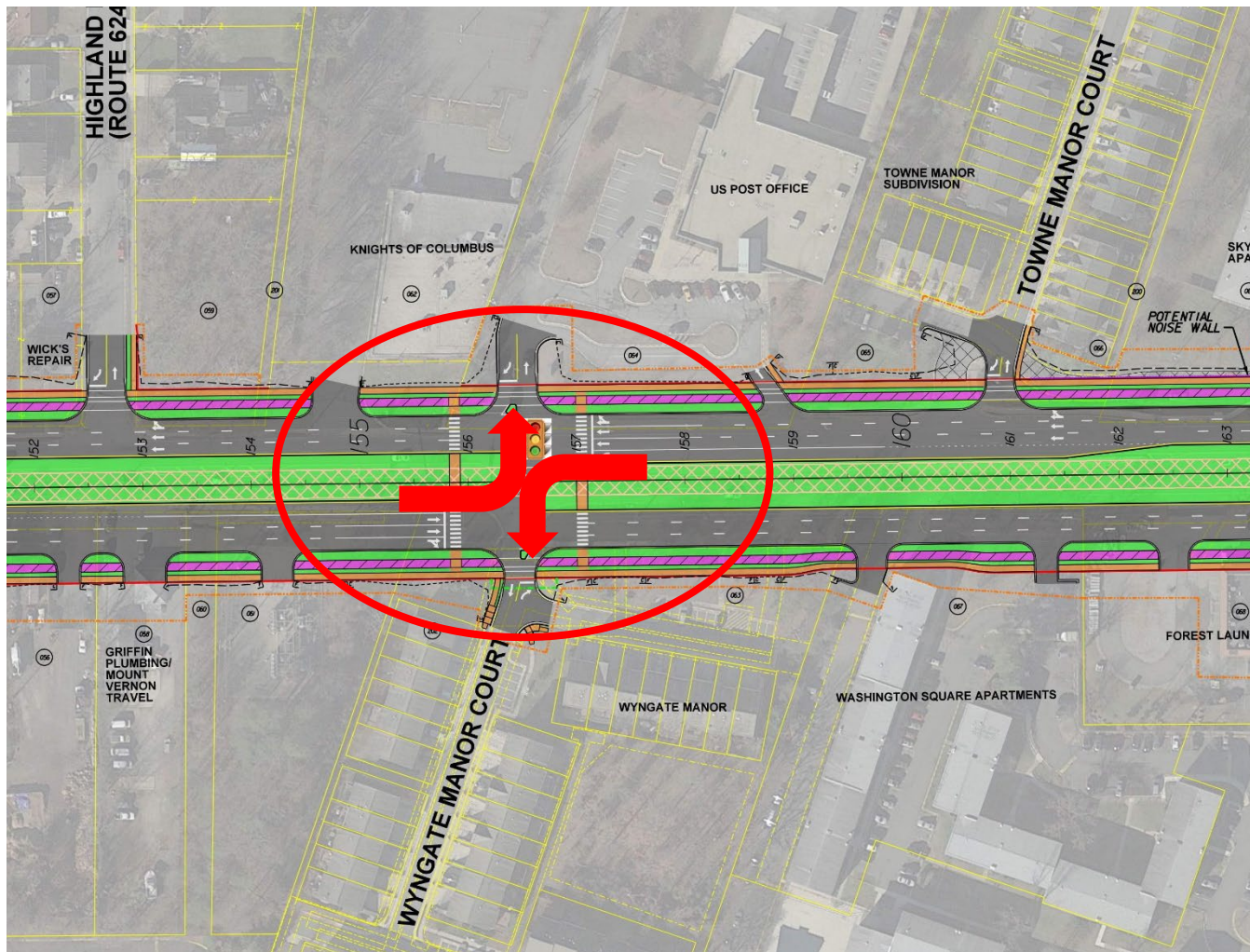
Intersection Improvements

Sacramento Drive / Cooper Road

- Realign Sacramento Drive to meet Cooper Road
- Single, traditional intersection
- Requires removal of several Woodlawn Center businesses
- Consistent with Fairfax County Comprehensive Plan



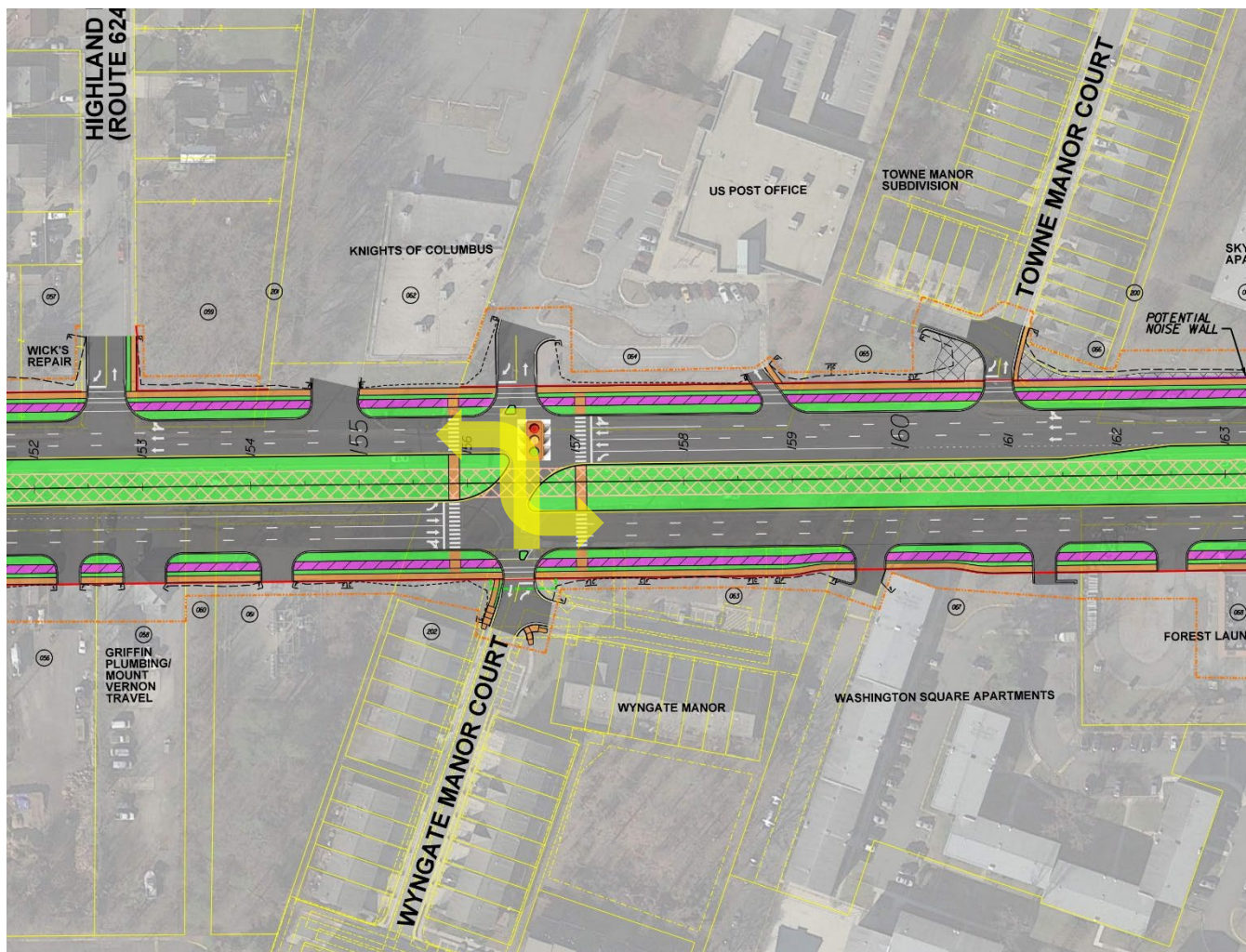
Intersection Improvements



Added Intersection **USPS Driveway / Wyngate Manor Court**

- Added signalized intersection – provides “lefts-in” to side streets

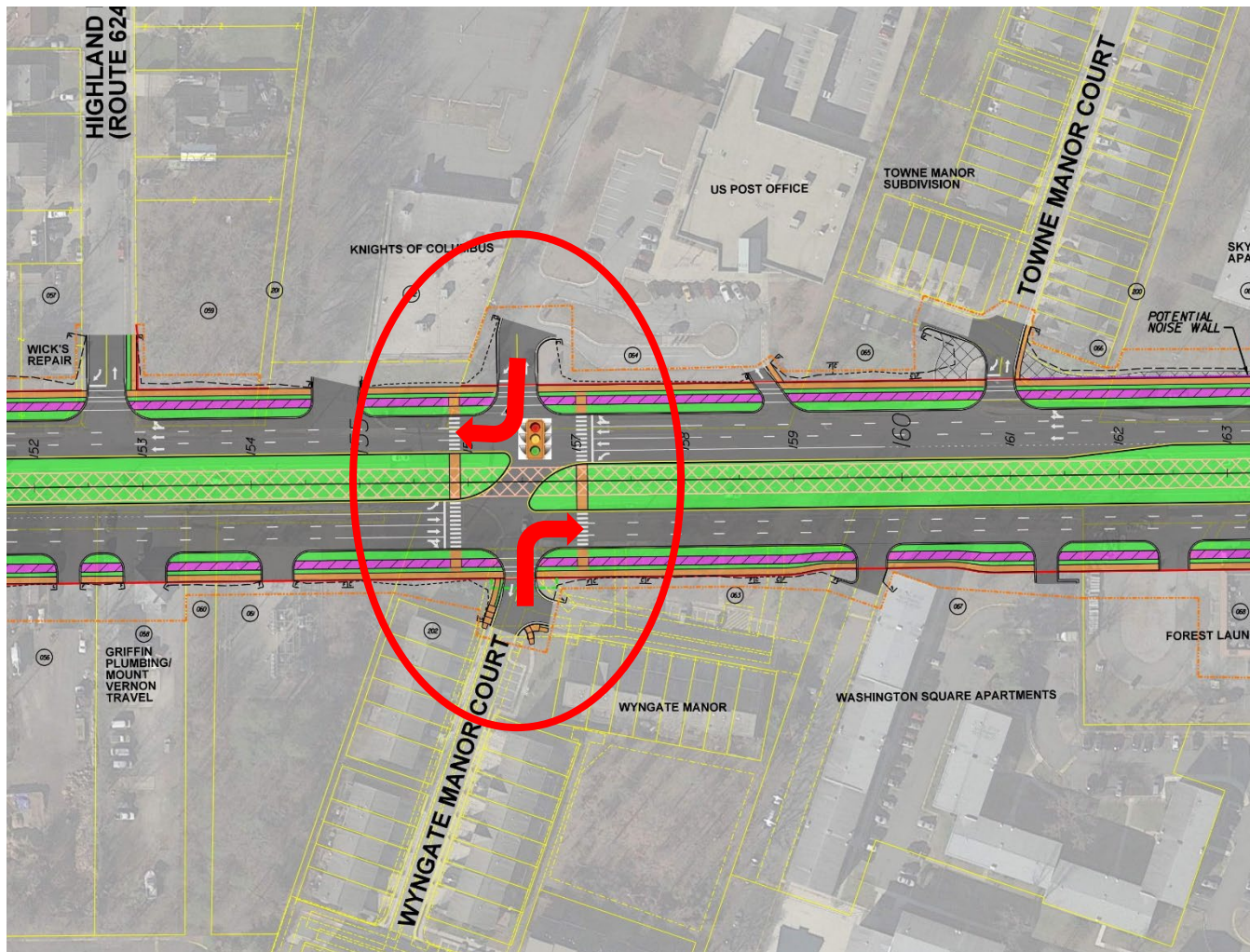
Intersection Improvements



Added Intersection **USPS Driveway / Wyngate** **Manor Court**

- Added signalized intersection – provides “lefts-in” to side streets
- “Lefts-out” prohibited

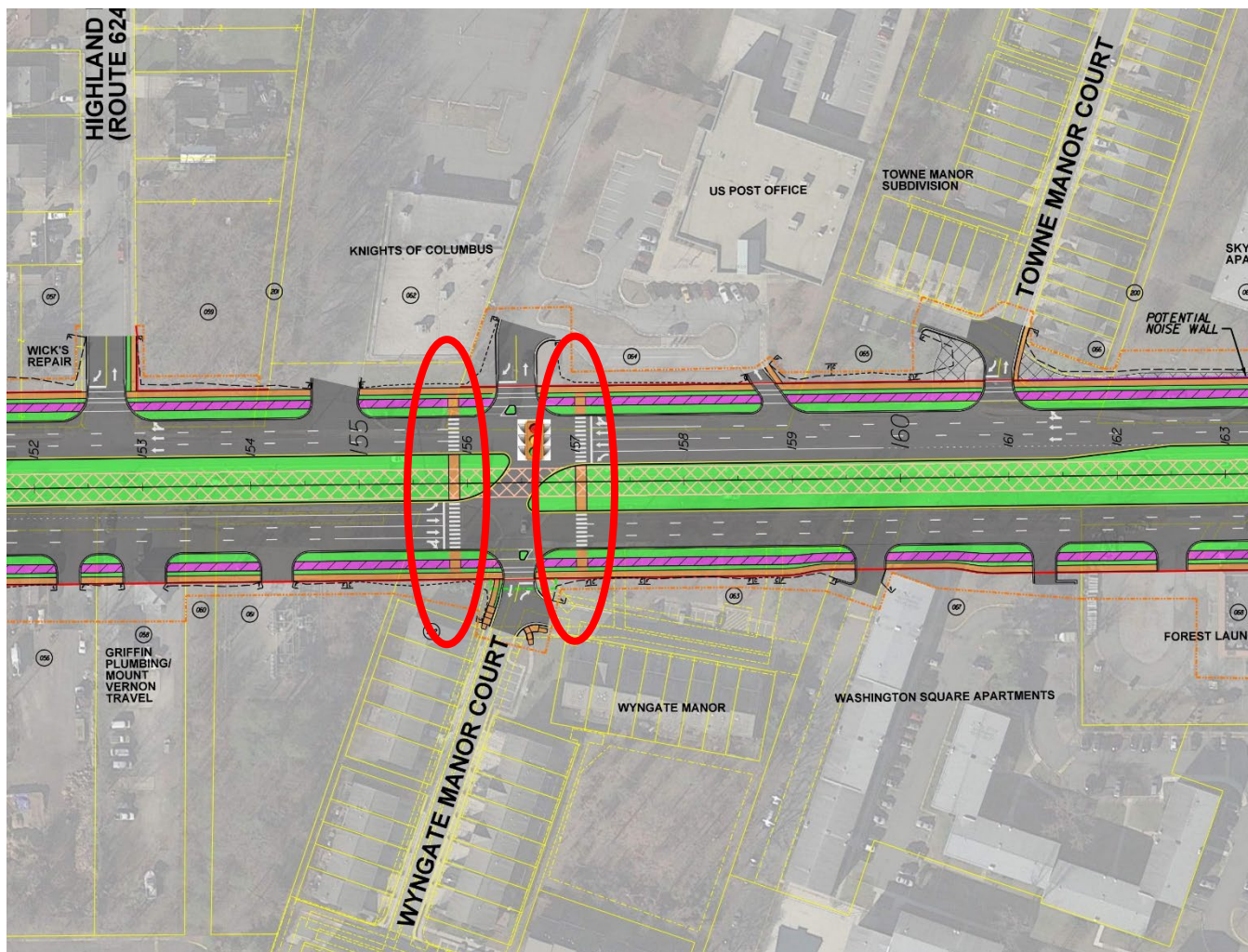
Intersection Improvements



Added Intersection **USPS Driveway / Wyngate Manor Court**

- Added signalized intersection – provides “lefts-in” to side streets
- “Lefts-out” prohibited
- “Right-out” only from side streets

Intersection Improvements

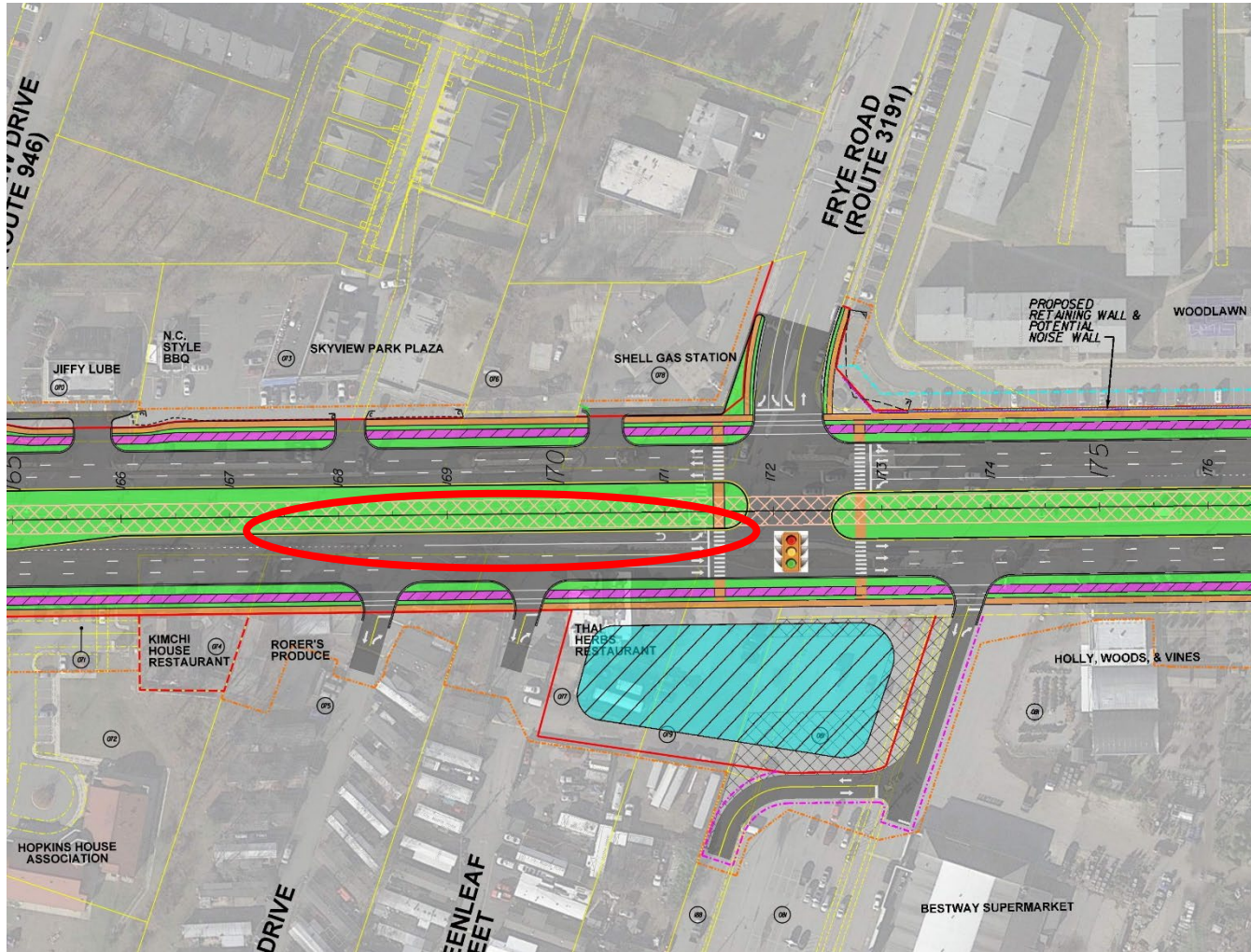


Added Intersection **USPS Driveway / Wyngate Manor Court**

- Added signalized intersection – provides “lefts-in” to side streets
- “Lefts-out” prohibited
- “Right-out” only from side streets
- Signalized pedestrian crossing



Intersection Improvements



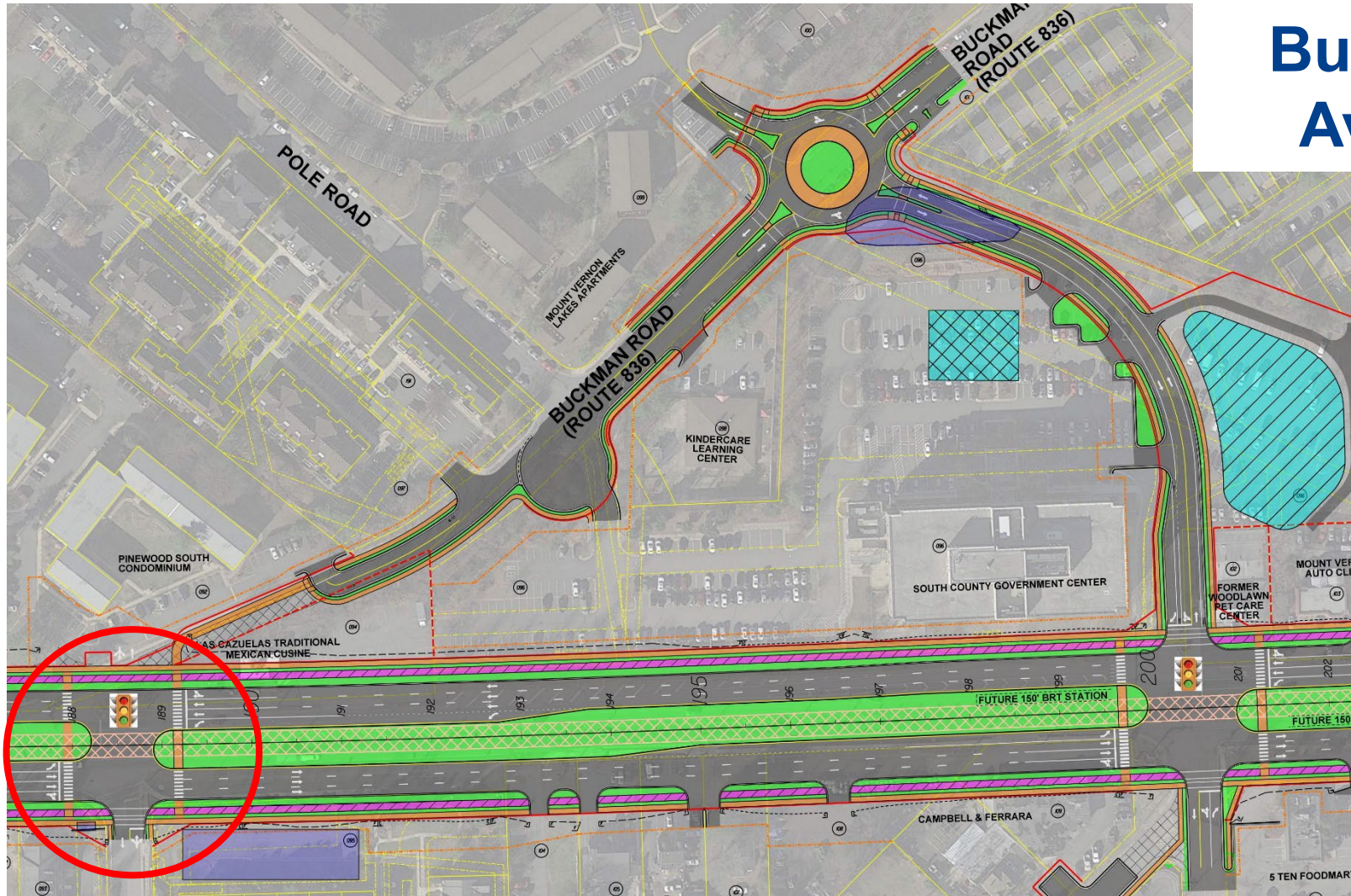
Modified Intersection **Frye Road / Bestway** **Entrance**

- **Single northbound left-turn lane (previously dual-left)**
- **Dual-left no longer needed with addition of USPS signal**
- **Realigning Master Roofing and Siding / D.A.M. Garage entrance for stormwater pond**

Intersection Improvements

Intersection Modifications Buckman Road / Radford Avenue / Mohawk Lane

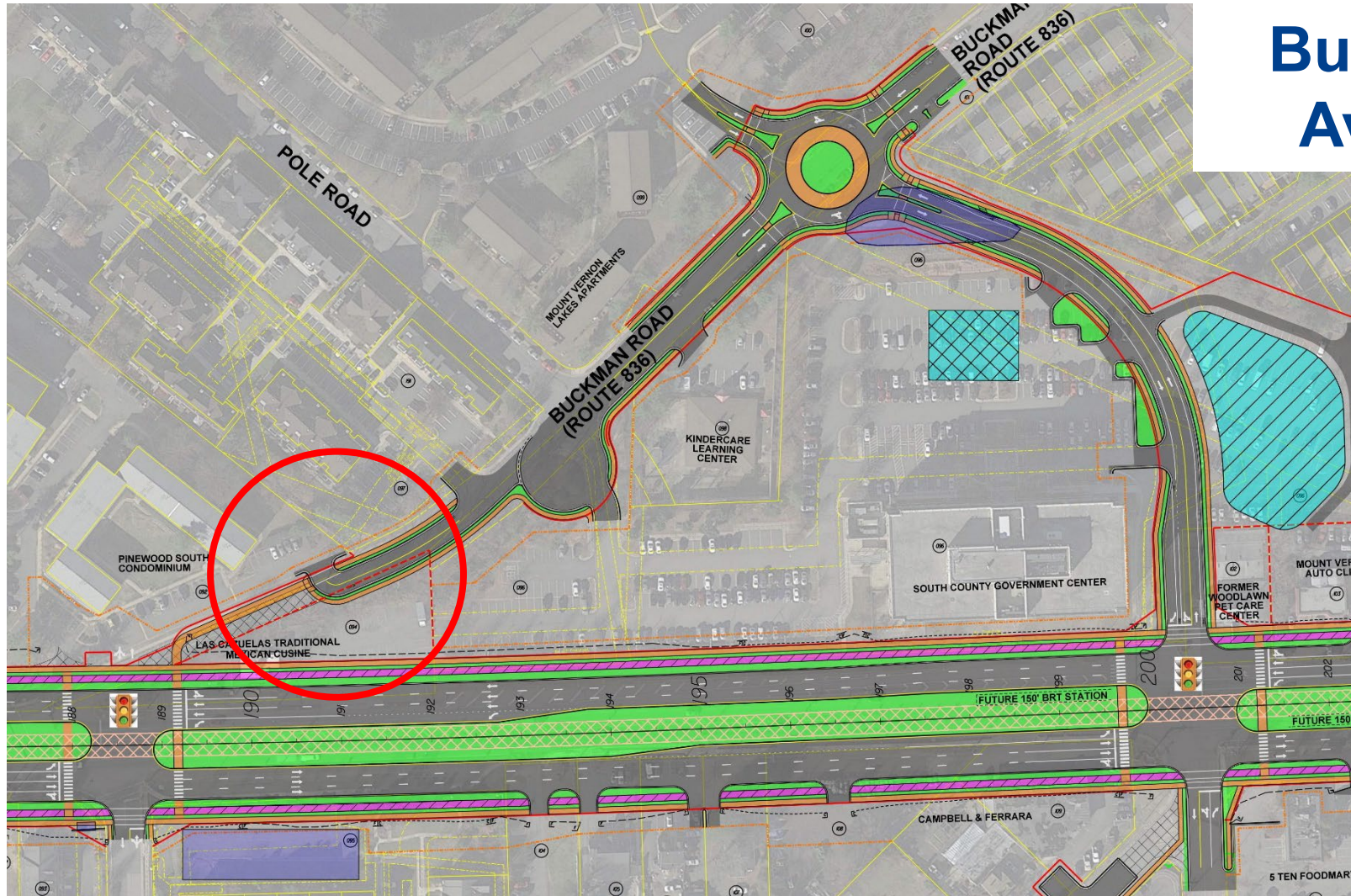
- Radford Avenue converted to “T” intersection



Intersection Improvements

Intersection Modifications Buckman Road / Radford Avenue / Mohawk Lane

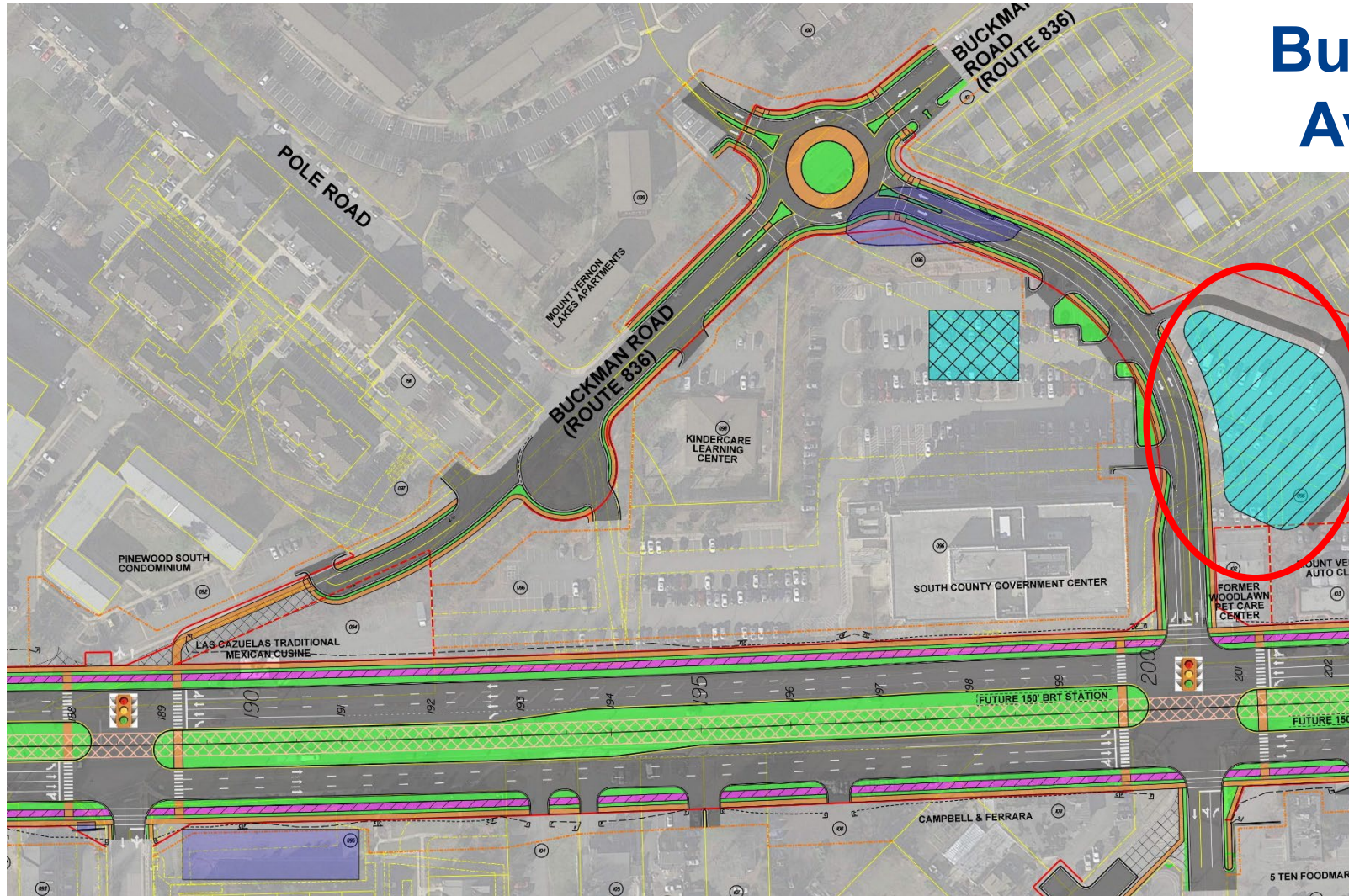
- Radford Avenue converted to “T” intersection
- Pinewood South Driveway relocated to Buckman Rd



Intersection Improvements

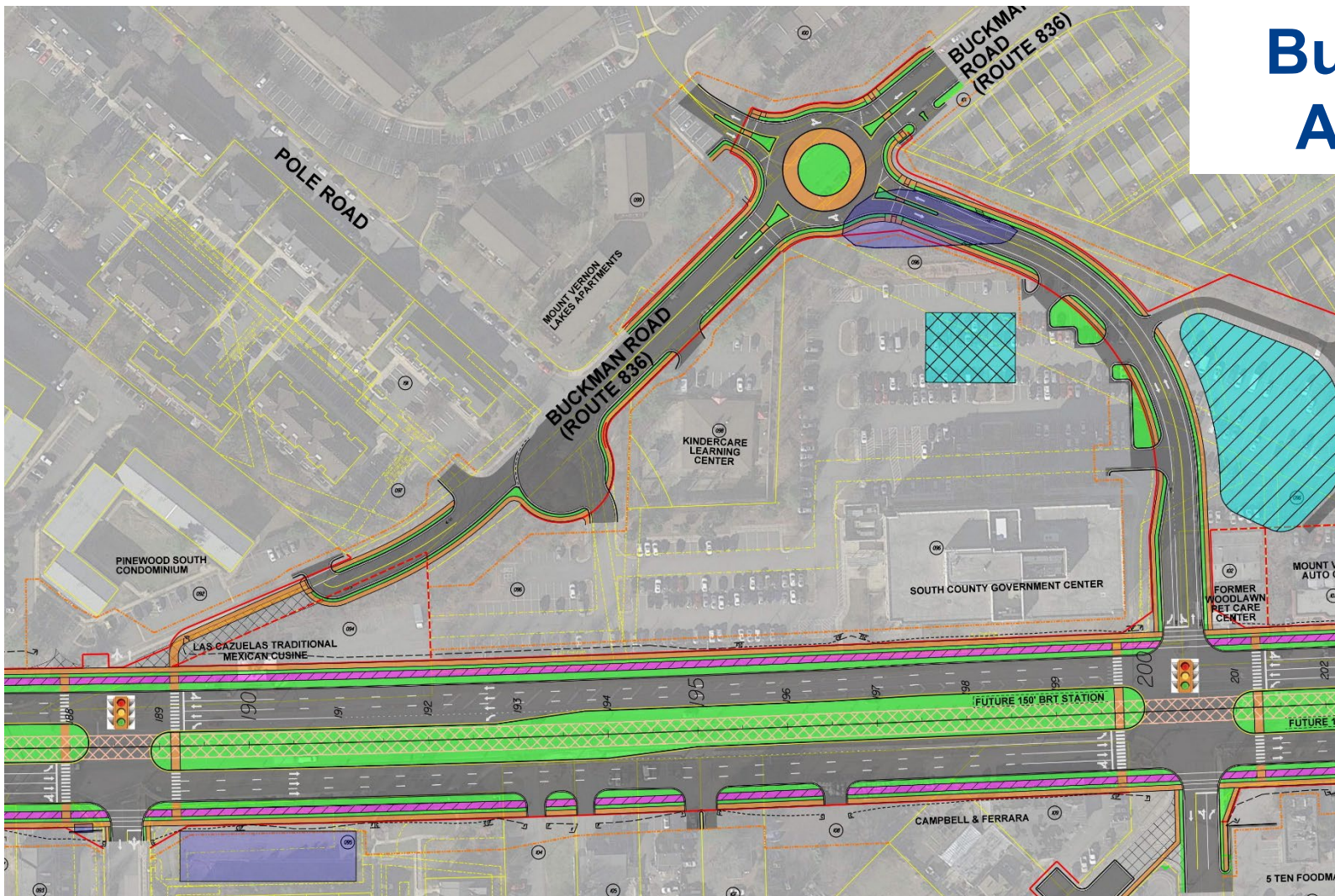
Intersection Modifications Buckman Road / Radford Avenue / Mohawk Lane

- Radford Avenue converted to “T” intersection
- Pinewood South Driveway relocated to Buckman Rd
- Stormwater pond added north of South County Government Center bldg



Intersection Improvements

Intersection Modifications Buckman Road / Radford Avenue / Mohawk Lane



- Radford Avenue converted to “T” intersection
- Pinewood South Driveway relocated to Buckman Rd
- Stormwater pond added north of South County Government Center bldg
- Realign Buckman Road to Mohawk Lane via roundabout
- Realign intersection to Radford Ave
- Cul-de-sac along Buckman Rd
- Close Washington Avenue at Mohawk Lane with turnaround

Intersection Improvements

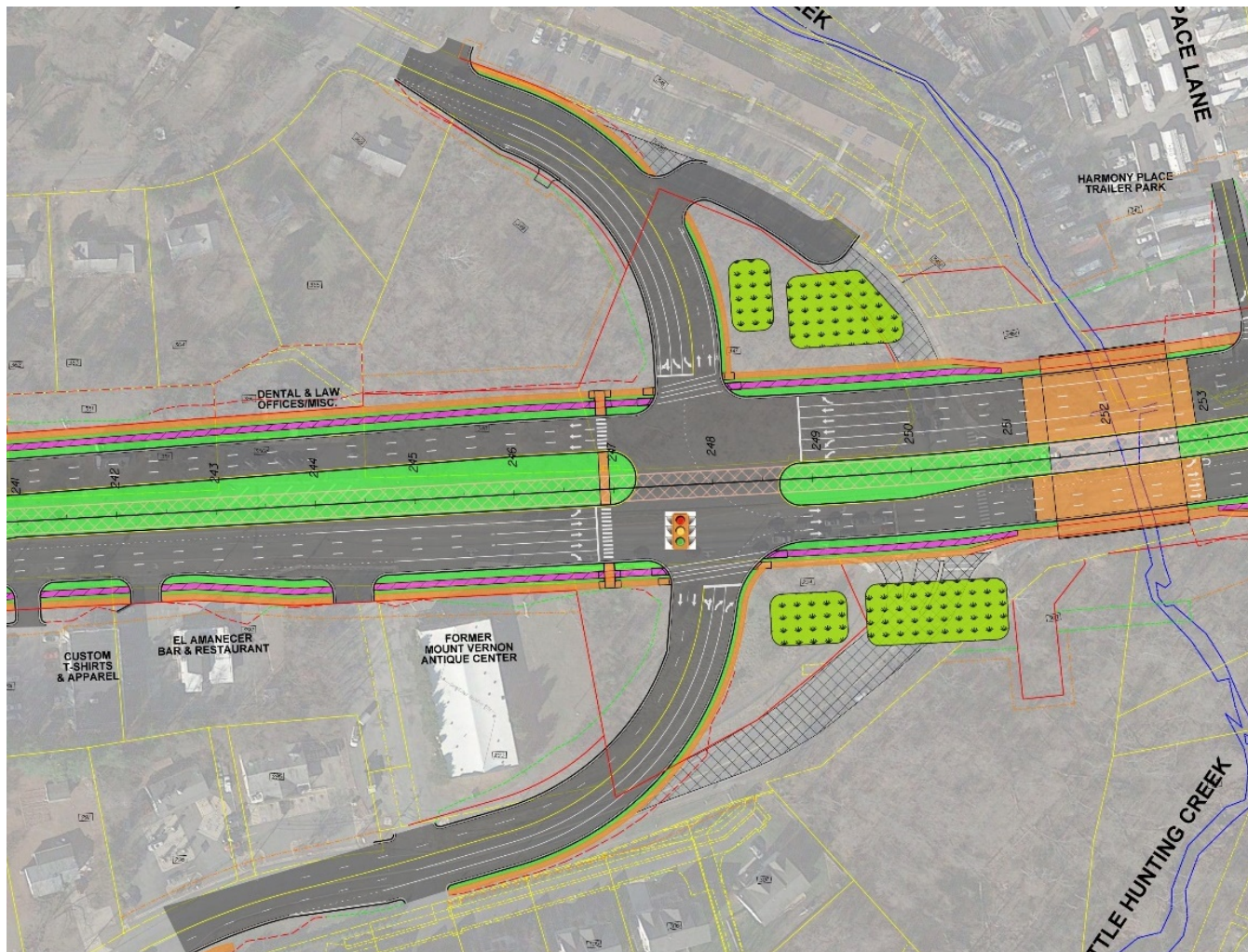
Russell Road / Reddick Avenue

- Realigning Reddick Avenue to align with Russell Road
- Dual left-turn lanes along Russell Road
- Single left-turn lane along Reddick Avenue



Intersection Improvements

Buckman Road / Mt. Vernon Highway



- Realign intersection 200 feet south of existing
- Dual left-turn lanes southbound to Mt. Vernon Highway
- Dual left-turn lanes from Buckman Road
- Dual right-turn lanes from Mt. Vernon Highway
- Pedestrian crossings across east, south and west legs of intersection

Pedestrian Underpasses

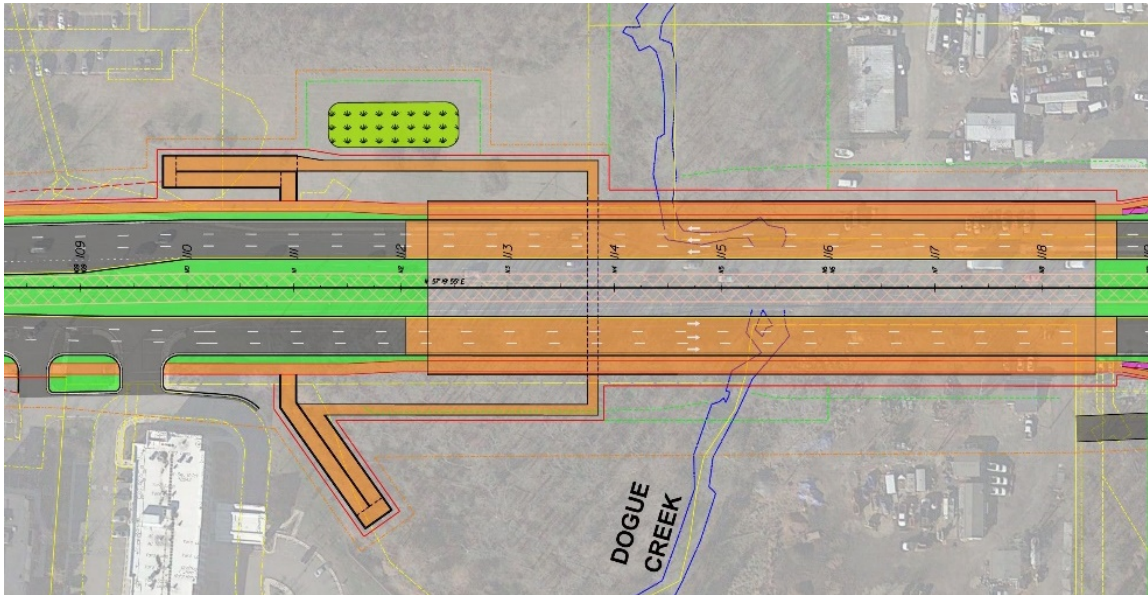


Dogue Creek and Little Hunting Creek

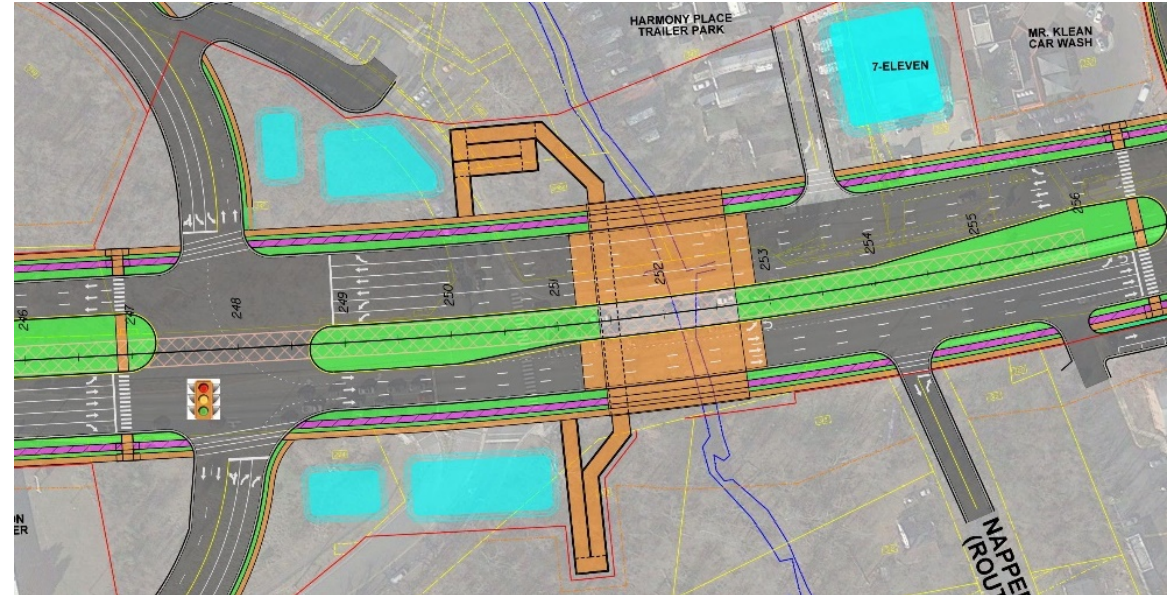
- Provides crossing of Richmond Highway at separate elevation
- No pedestrian/vehicle conflicts
- Passes under road adjacent to creeks
- Ramps provided for pedestrian access
- Lighting will be included

Pedestrian Underpass

Dogue Creek

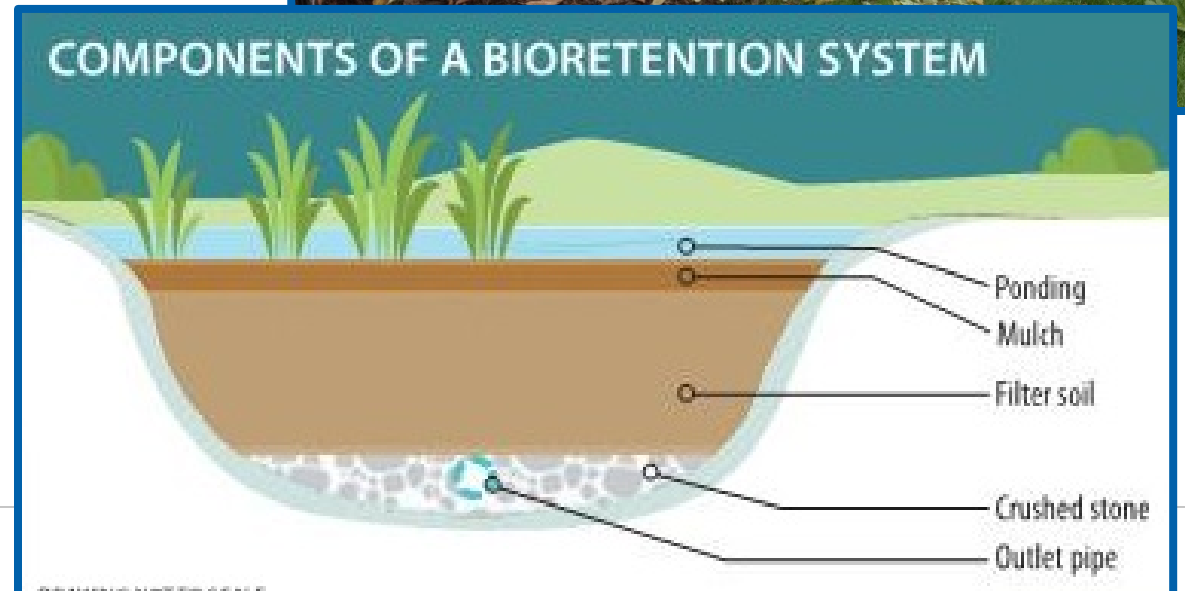


Little Hunting Creek



Stormwater Management

VDOT is required by law to control and treat stormwater runoff from roadways. New, more stringent regulations took effect in July 2014, requiring more sophisticated stormwater treatment facilities, especially in a constrained urban corridor such as Richmond Highway. These regulations require ponds to be bigger and to detain more water for longer periods of time. Therefore, more and larger facilities are required for roadway projects





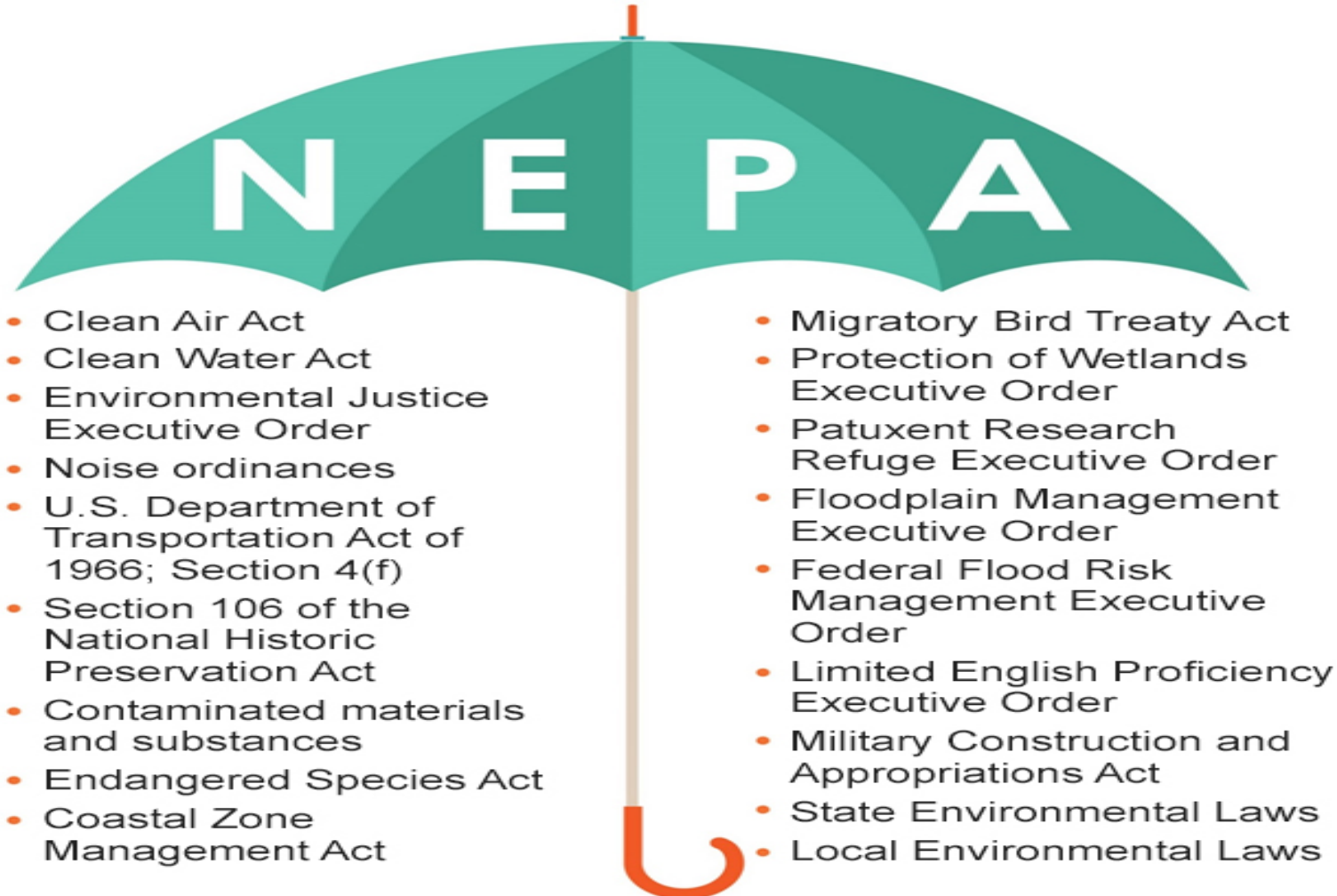
Stormwater Management Detention Examples

- Bioretention
- Wet Pond



Final facility type, location and size to be determined in detailed design.

Environmental Assessment National Environmental Policy Act



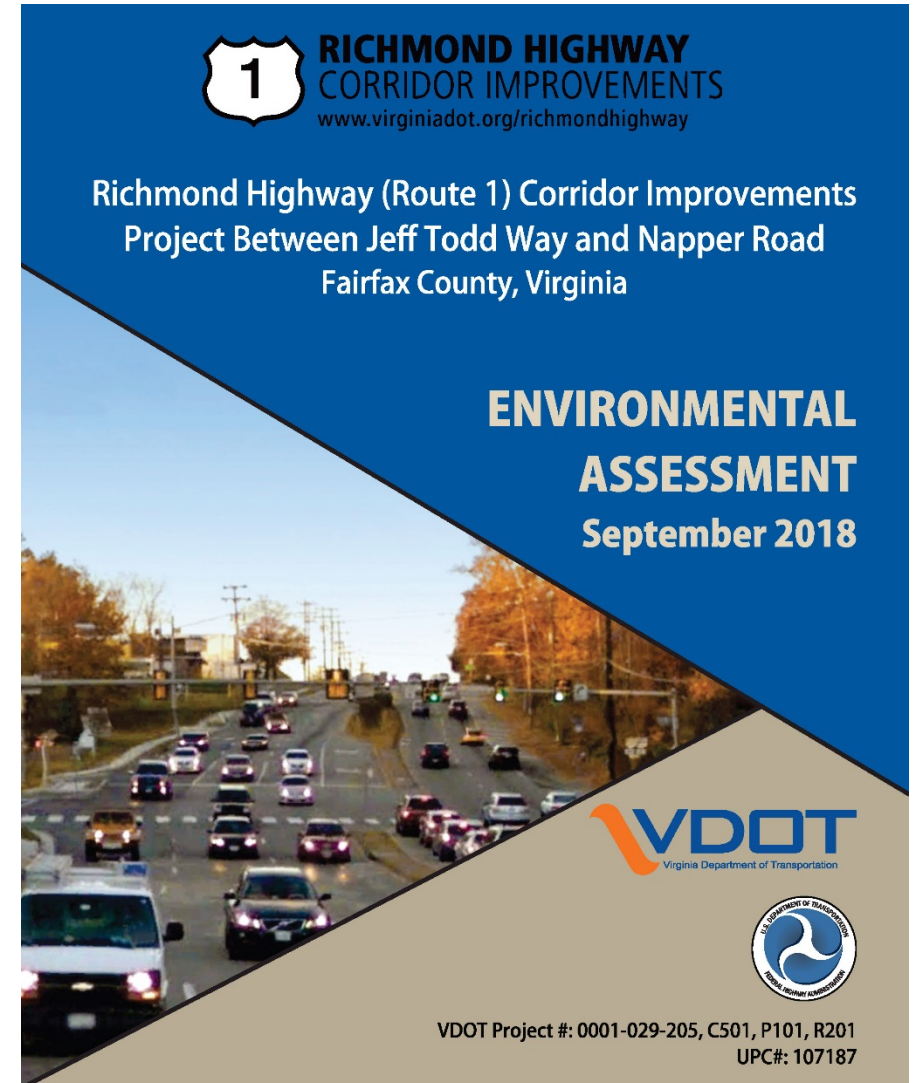
Environmental Assessment


A NEPA public hearing was held Oct. 29, 2018 and the comment period closed Dec. 6, 2018.

EA Document is available on the website for review.

Final Step in NEPA Process:


- Addressing Comments received
- Seeking Finding of No Significant Impact from FHWA, in Winter 2019/2020.




 **RICHMOND HIGHWAY**
CORRIDOR IMPROVEMENTS
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Richmond Highway (Route 1) Corridor Improvements
Project Between Jeff Todd Way and Napper Road
Fairfax County, Virginia

**ENVIRONMENTAL
ASSESSMENT**
September 2018

 **VDOT**
Virginia Department of Transportation



VDOT Project #: 0001-029-205, C501, P101, R201
UPC#: 107187

Noise Analysis

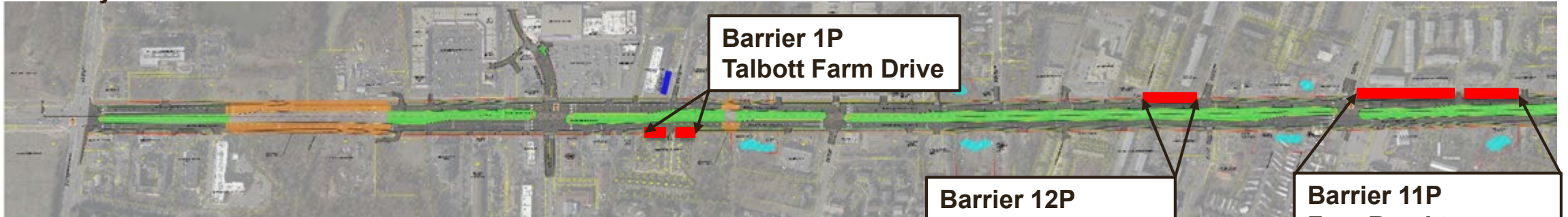
- ⇒ **Required to evaluate noise levels on federally funded projects to comply with federal law under the National Environmental Policy Act**
- ⇒ **Where project noise levels are projected to exceed established criteria, VDOT is required to propose noise mitigation**
- ⇒ **Sound Barriers will be constructed only if the people who are directly benefited vote for them**
- ⇒ **Noise Analysis**
 - **Preliminary and Final**
 - **Computer model calibrated to existing conditions**
 - **Based on design year traffic volumes (2045)**
 - **Loudest hour – 3 PM for Richmond Highway**

Preliminary Noise Analysis

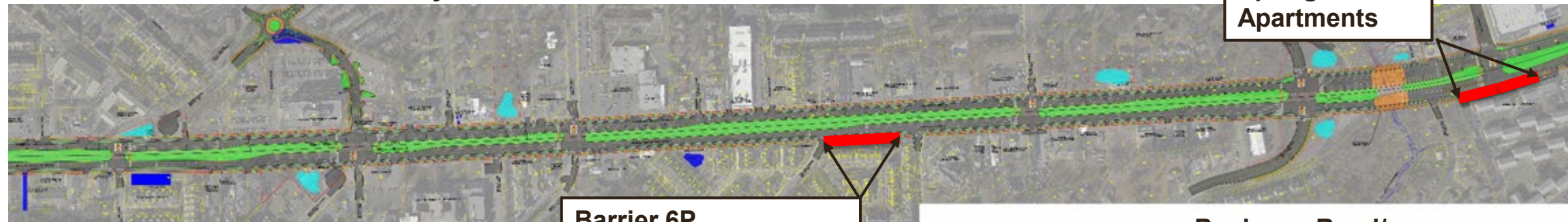
Jeff Todd
Way

Sacramento
Drive

Frye
Road



- NB Rte. 1 – Barriers 1P, 6P, 7P – 1,009'
- SB Rte. 1 – Barriers 11P, 12P – 1,004'
- Approximately 6.5% of total project length
- Aesthetic treatments may be considered



Buckman Road/
Mohawk Lane

Barrier 6P
Central Avenue to
Shannons Green Way

Buckman Road/
Mt. Vernon Hwy

Barrier 7P
Spring Garden
Apartments

Example – Sound Barrier Voting

Voting for Barrier G12	
Color	# of Benefited Receptors
Yellow	8
Blue	1

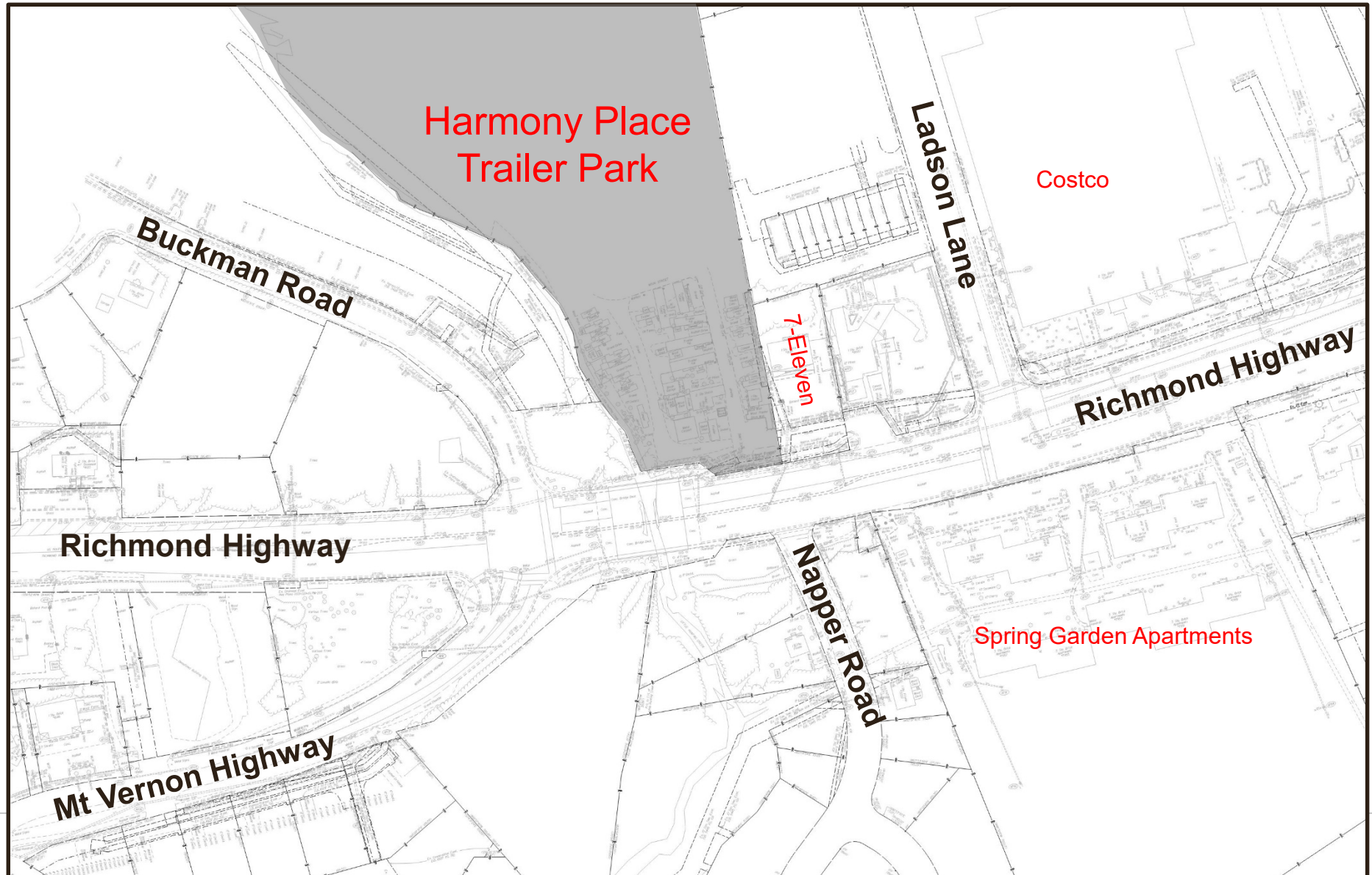
⇒ **Only benefited receptors vote**

- Yellow - Impacted and Benefited is weighted as a 5
- Blue - Benefited but not impacted is weighted as a 3
- Green – Not Benefited and not impacted – Do not vote
- Rentals – Owner and renter votes
- Voting occurs during Final Design 2019 - 2022

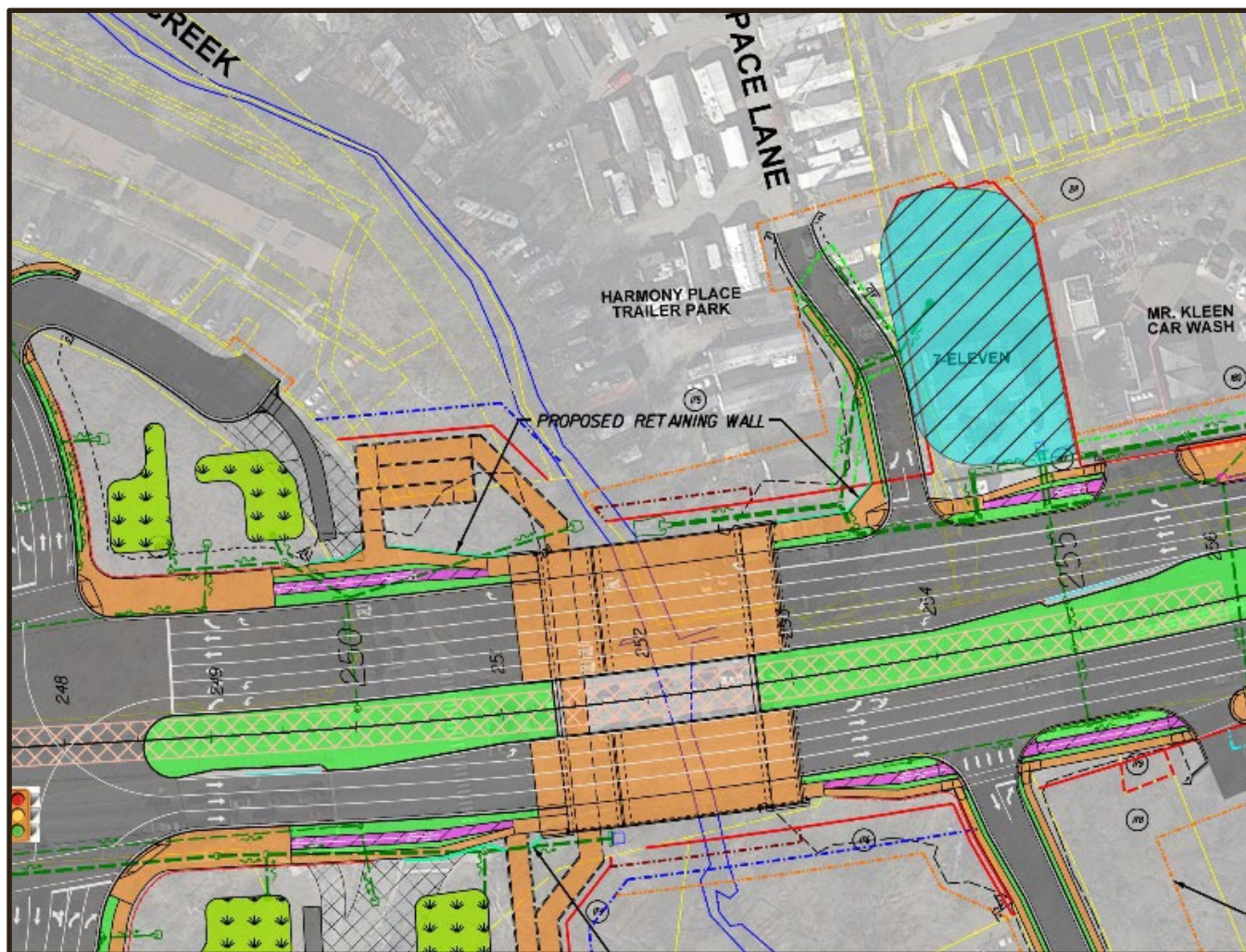




Harmony Place Trailer Park



Harmony Place Trailer Park

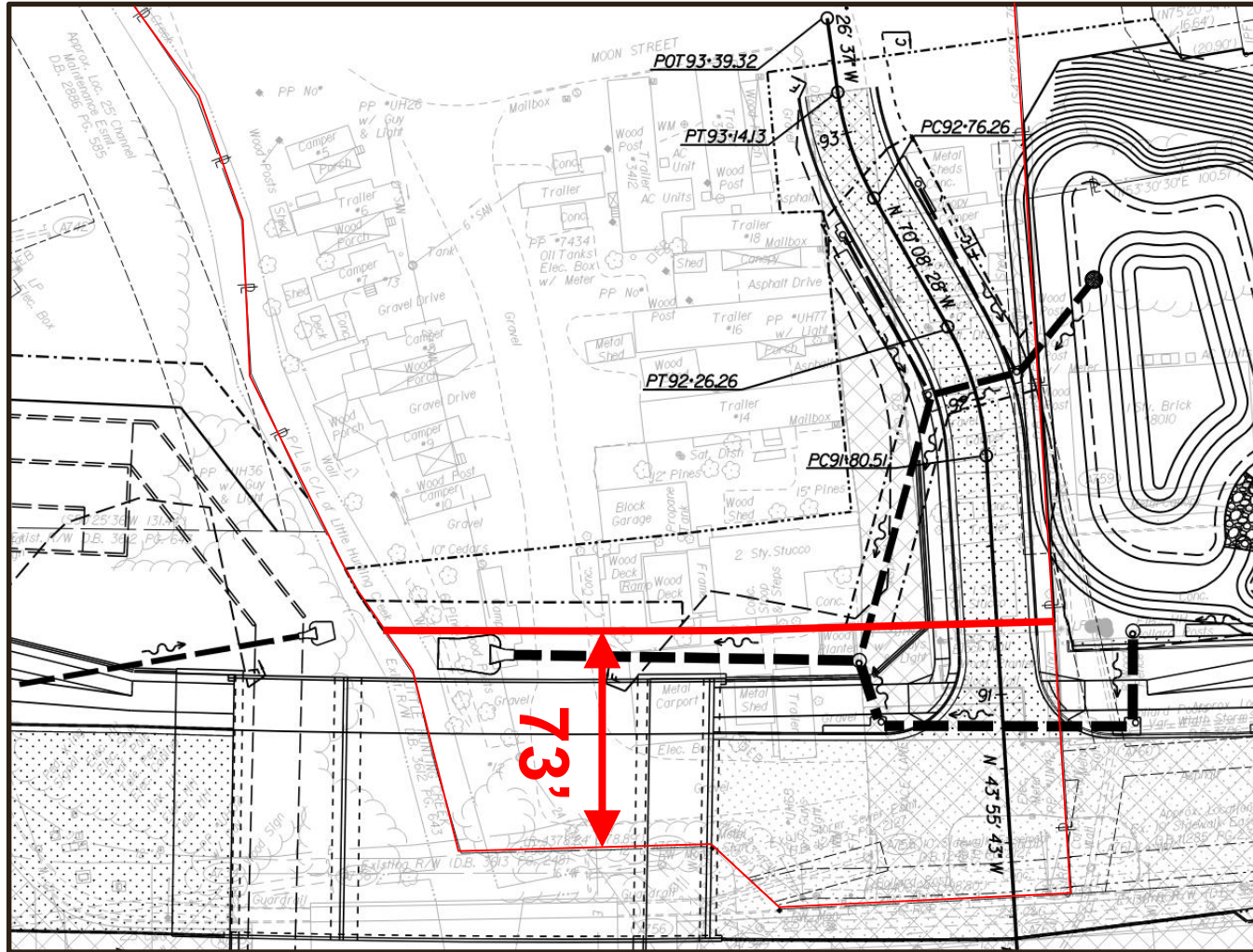


Proposed Roadway Changes

- Roadway widening and improvement along frontage
- Two additional traffic lanes
- Sidewalk/Cycle Track improved
- Pace Lane shifted
- Right-In/Right-out driveway
- Retaining walls constructed
- Some home sites removed
- Adjacent 7-Eleven store removed
- Stormwater pond installed
- New storm drainage system
- Proposed pedestrian underpass on other side of Little Hunting Creek



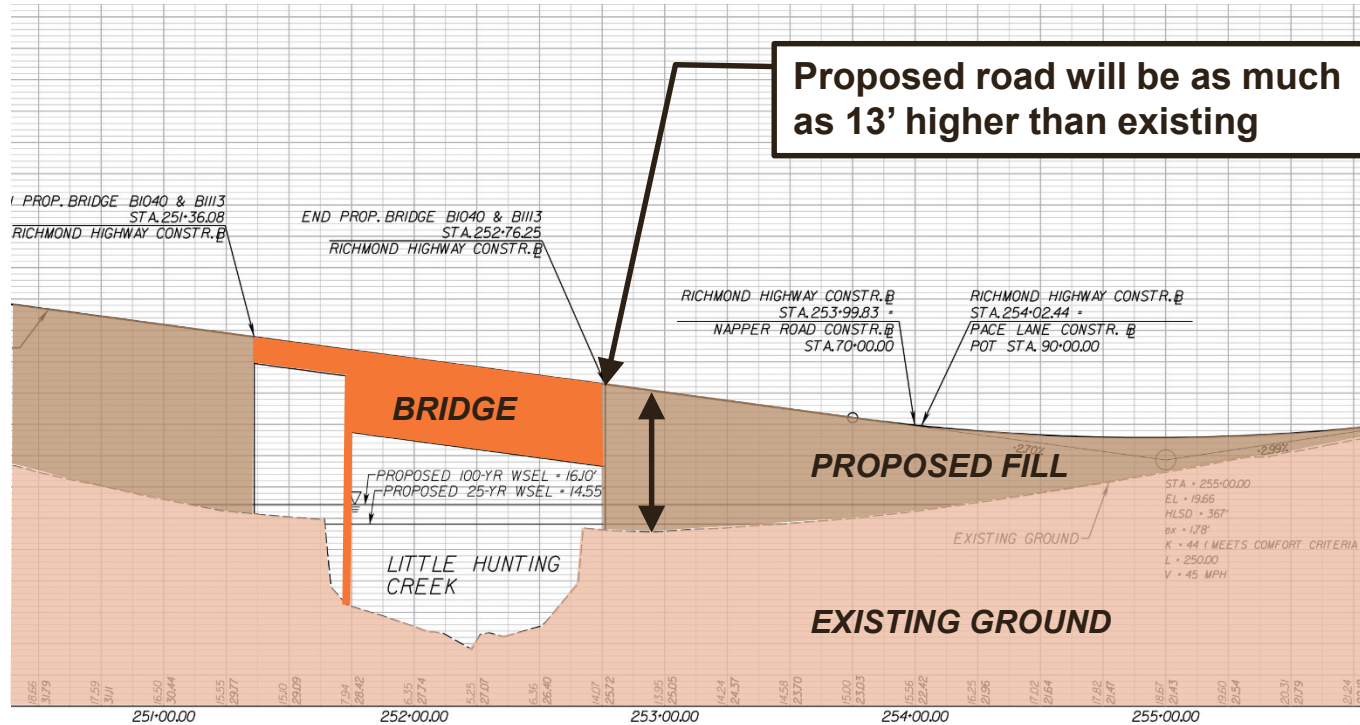
Harmony Place Trailer Park



Impacts to Harmony Place

- Roadway and cycle track/ sidewalk 73' closer to homes

Harmony Place Trailer Park

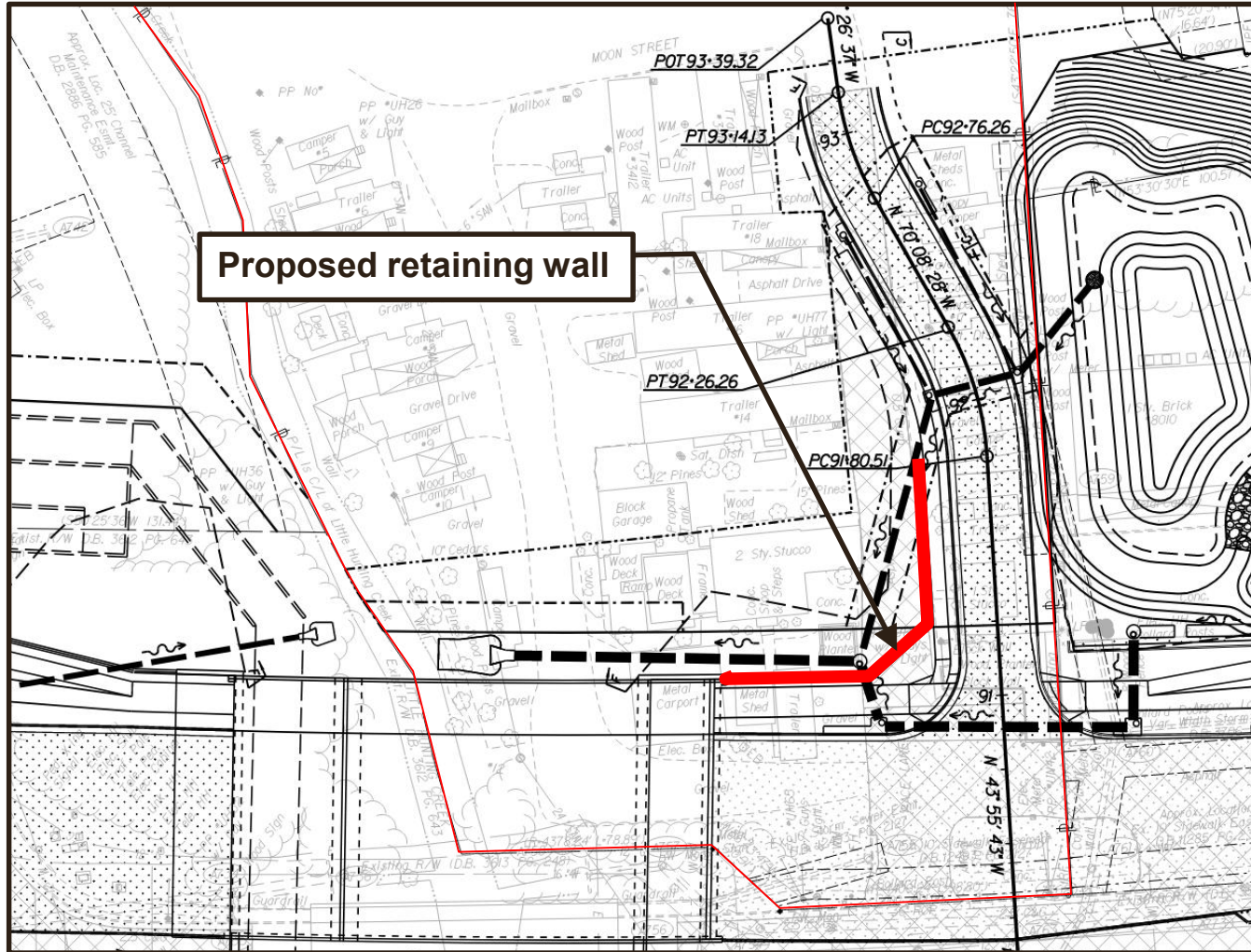


Impacts to Harmony Place

- Roadway and cycle track/
sidewalk 73' closer to homes
- Road elevation up to 13' higher
than existing road



Harmony Place Trailer Park

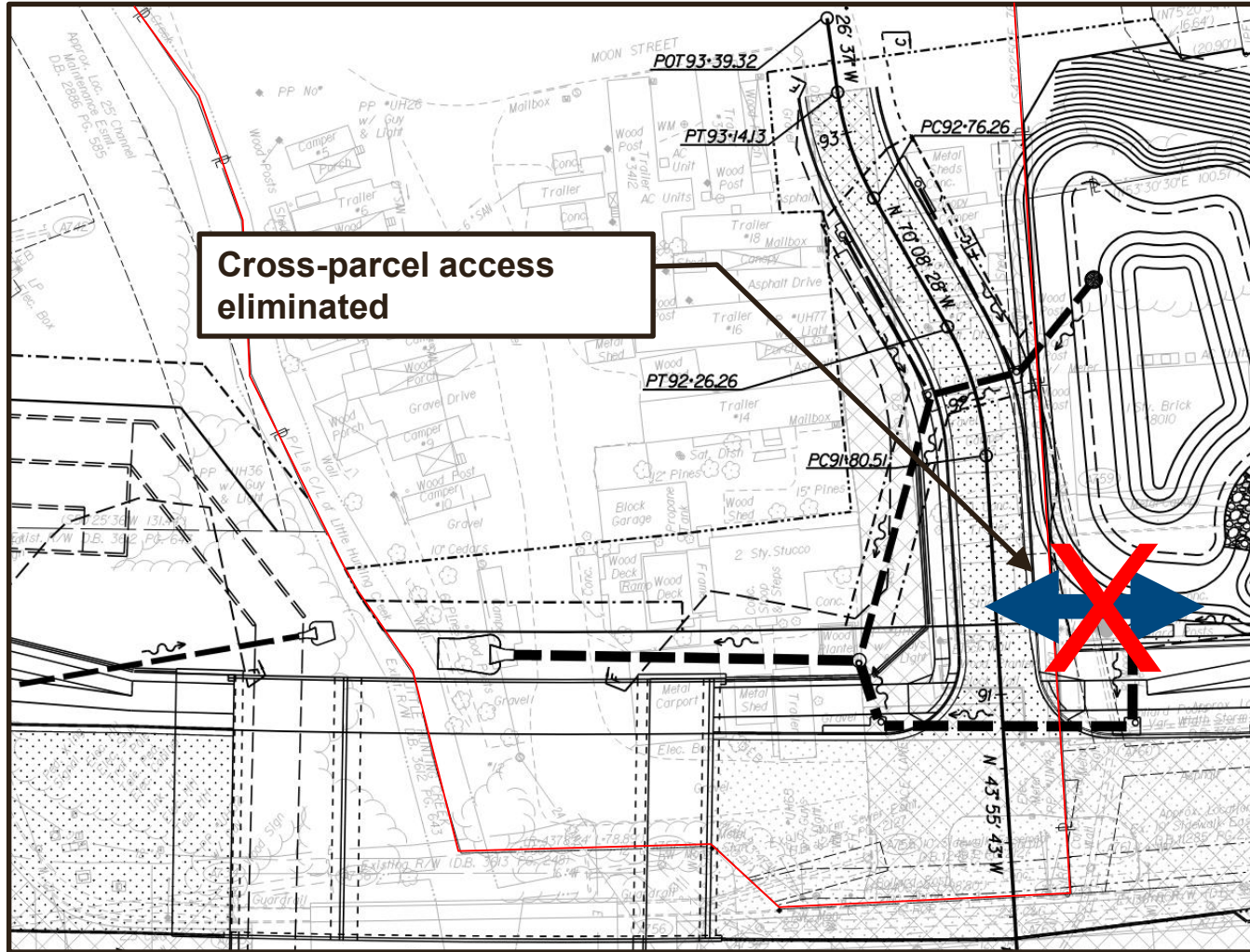


Impacts to Harmony Place

- Roadway and cycle track/sidewalk 73' closer to homes
- Road elevation up to 13' higher than existing road
- Retaining walls constructed to support road and driveway



Harmony Place Trailer Park



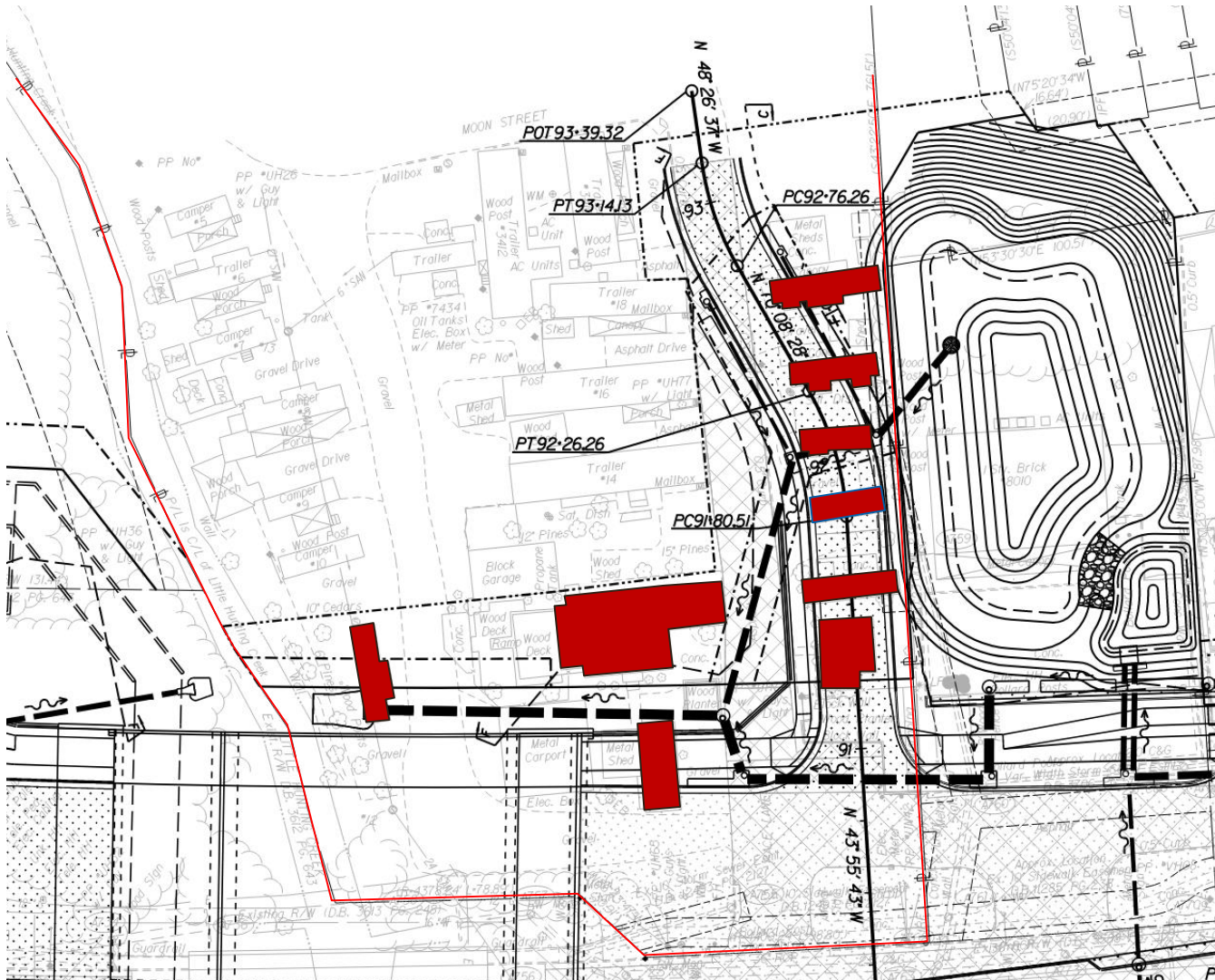
Impacts to Harmony Place

- Roadway and cycle track/sidewalk 73' closer to homes
- Road elevation up to 13' higher than existing road
- Retaining walls constructed to support road and driveway
- **Access drive to 7-Eleven eliminated**



Harmony Place Trailer Park

Impacts to Harmony Place



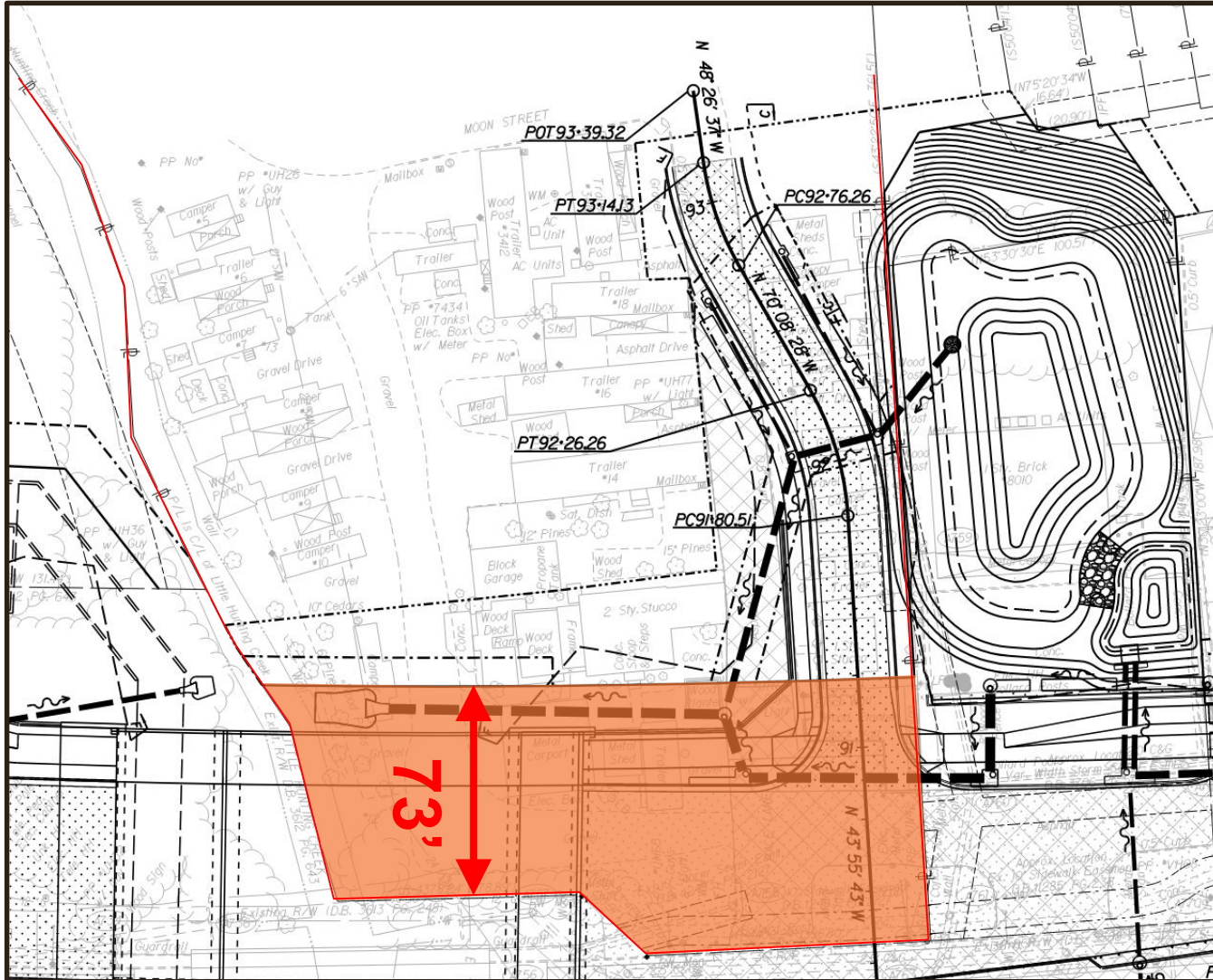
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- Road elevation up to 13' higher than existing road
- Retaining walls constructed to support road and driveway
- Access drive to 7-Eleven eliminated
- Some home sites removed



Harmony Place Trailer Park

Right-of-Way and Easements

- **New Permanent Right-of-Way**

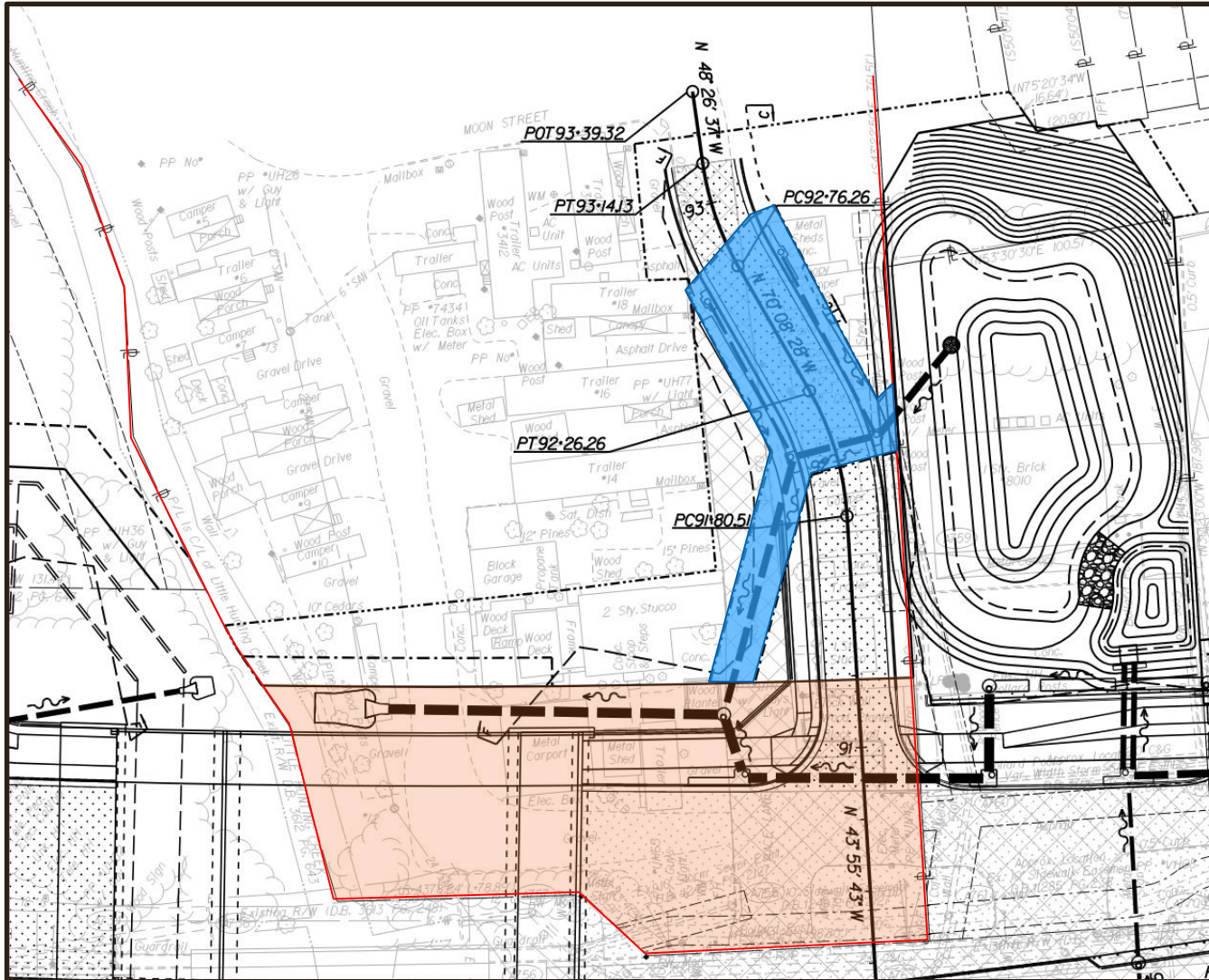




Harmony Place Trailer Park

Right-of-Way and Easements

- New Permanent Right-of-Way
- Permanent Drainage Easements for storm drains

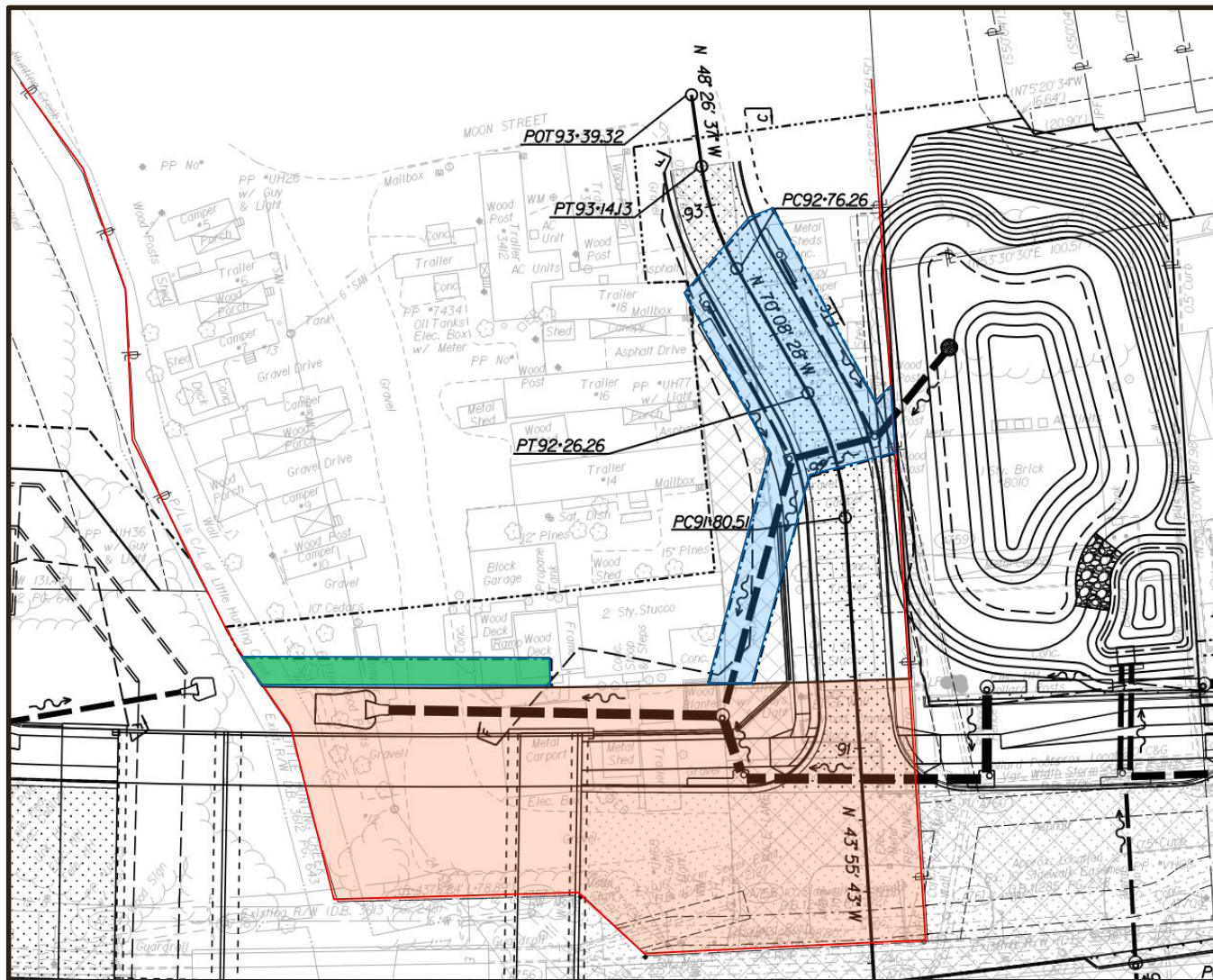




Harmony Place Trailer Park

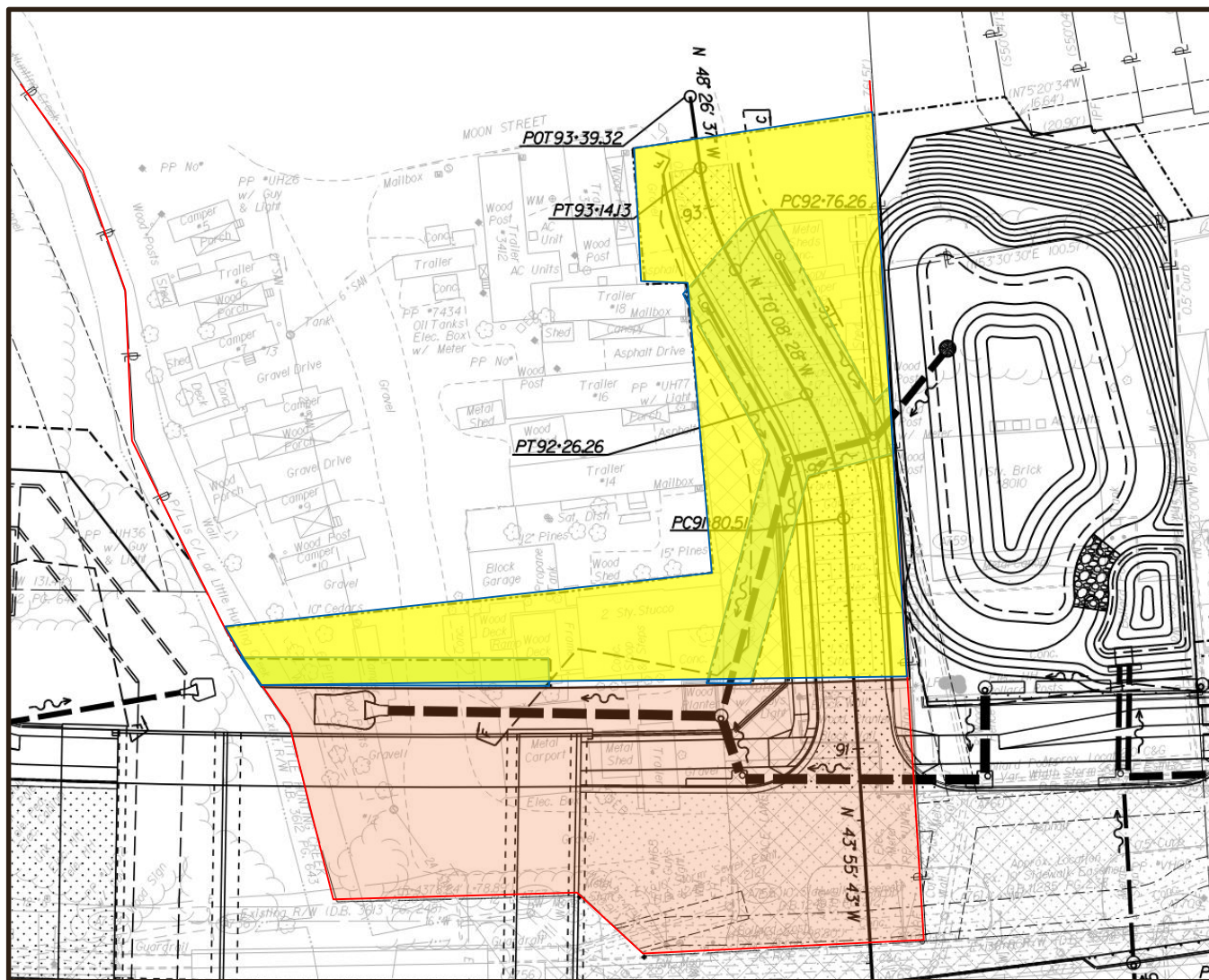
Right-of-Way and Easements

- New Permanent Right-of-Way
- Permanent Drainage Easements for storm drains
- **Permanent Utility Easement for utility relocations**



Harmony Place Trailer Park

Right-of-Way and Easements



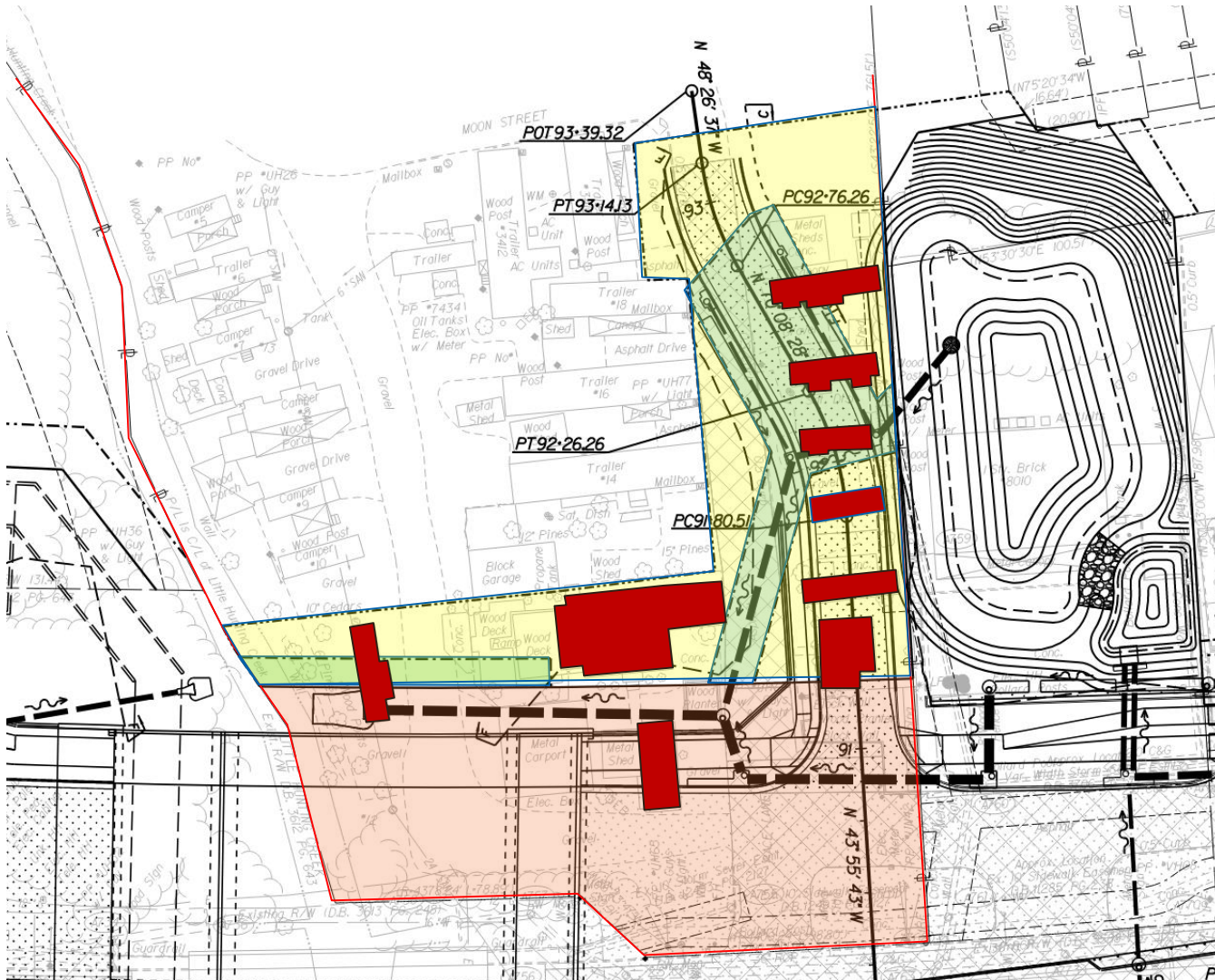
- New Permanent Right-of-Way
- Permanent Drainage Easements for storm drains
- Permanent Utility Easement for utility relocations
- **Temporary Construction Easement for project construction**



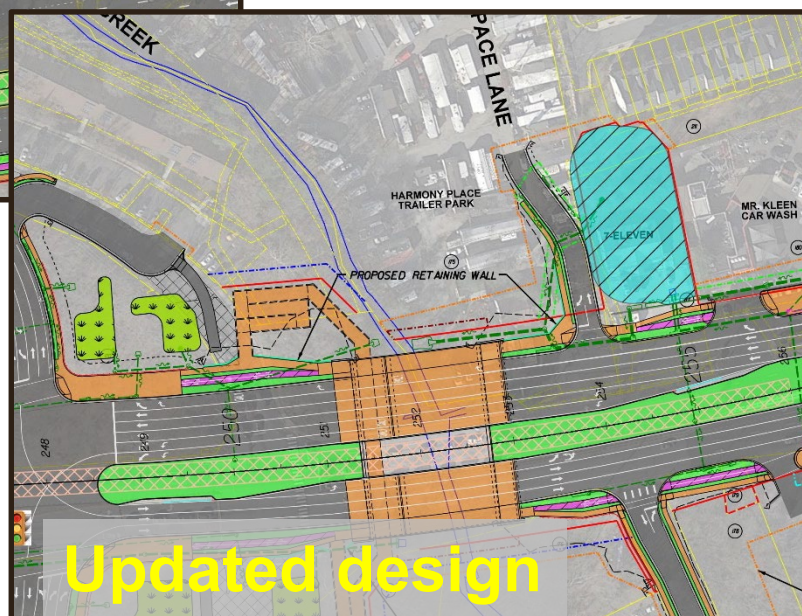
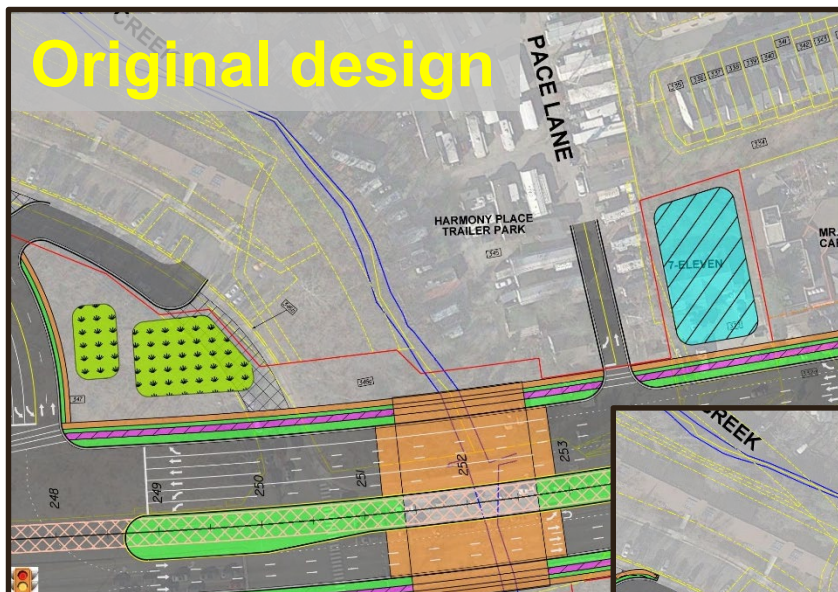
Harmony Place Trailer Park

Right-of-Way and Easements

- New Permanent Right-of-Way
- Permanent Drainage Easements for storm drains
- Permanent Utility Easement for utility relocations
- Temporary Construction Easement for project construction
- **9 home sites removed**



Harmony Place Trailer Park

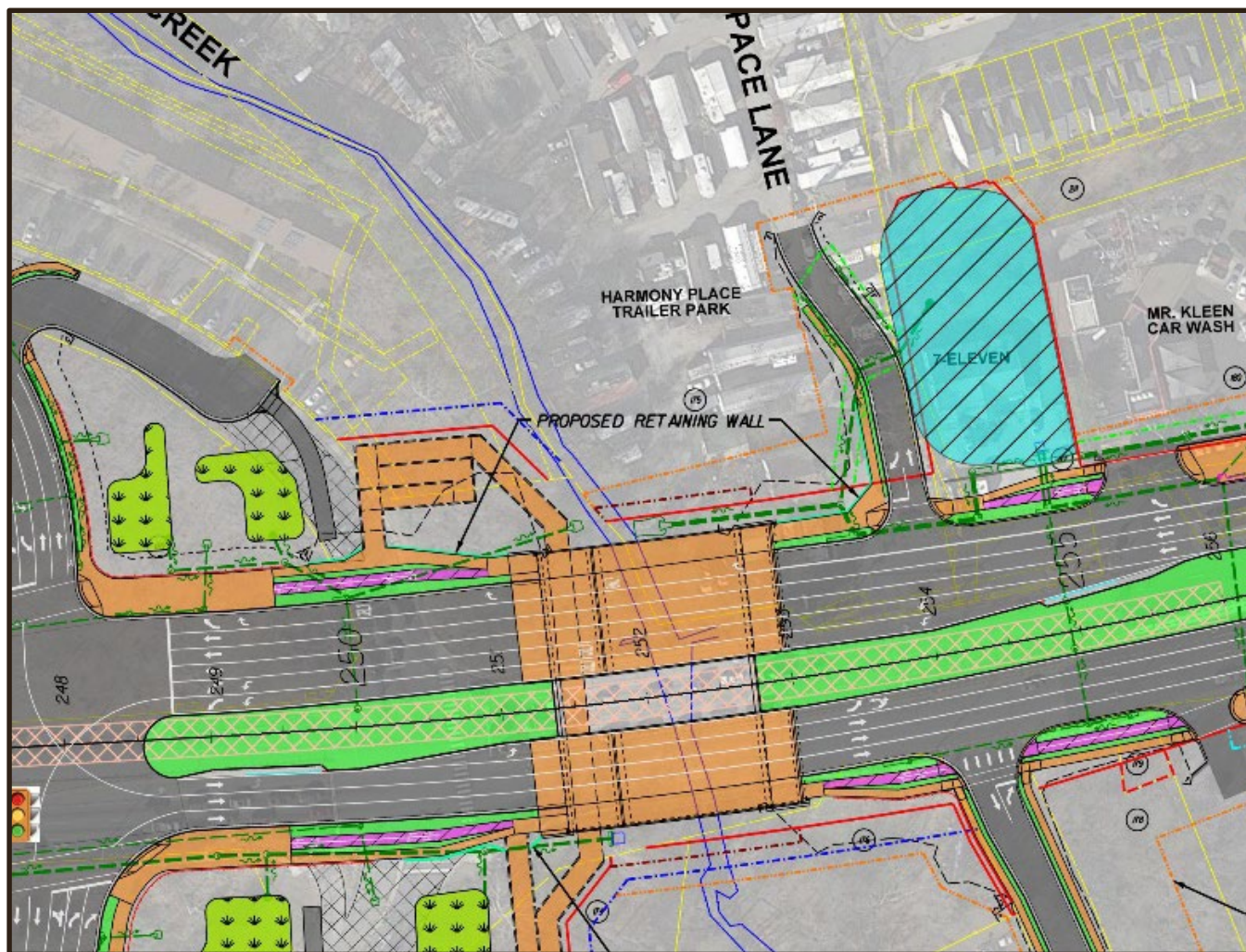


Design Adjustments to Harmony Place

Relocated Pace Lane

- Placed at road low point – reduces grades
- Three fewer homes taken
- Permanent and temporary easements reduced

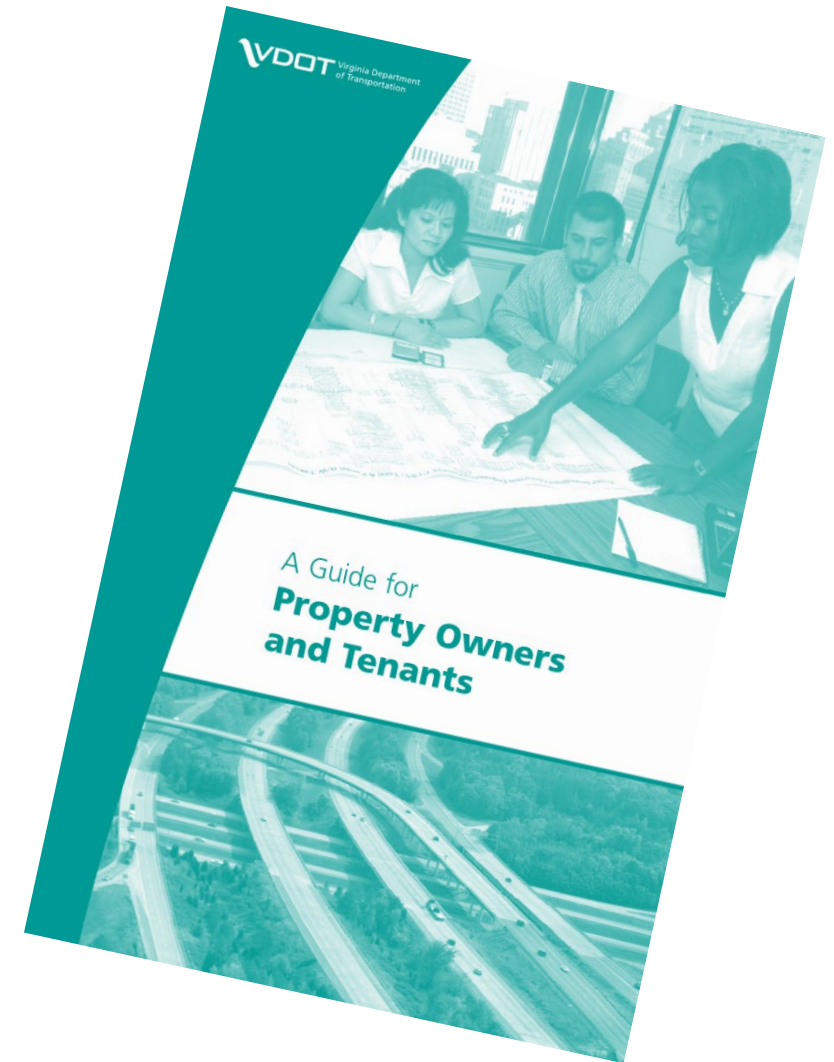
Harmony Place Trailer Park



- ## Benefits to Harmony Place
- Less congested and safer Richmond Highway
 - Continuous sidewalk and cycle tracks along Richmond Highway for the entire project length
 - Pedestrian signals and crosswalks at all signalized intersections.
 - Larger bridge opening will reduce flood potential/risk of debris blockage
 - Accommodates future Bus Rapid Transit.

Right-of-Way Acquisition

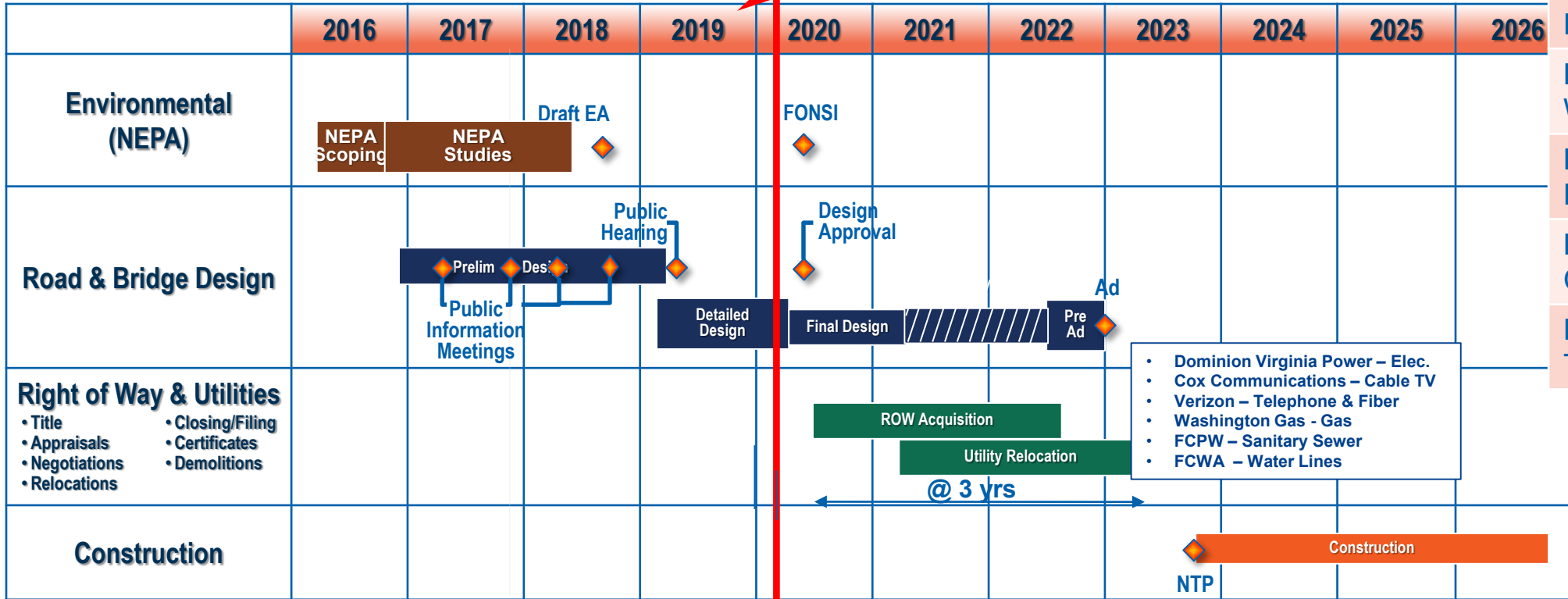
- ***Each property impact is unique***
- **Further design details are needed to determine full right of way impacts**
- **Right of way acquisition process to begin after detailed design – anticipated in mid-2020**
- **Meetings with individual property owners**
- **Right of Way representatives will be at each public meeting**



Schedule

**We are here -
February 6, 2020**

Calendar Schedule



Milestone Activities	Dates
Design Public Hearing	March 26, 2019
Begin Right of Way Acquisition	Mid 2020
Begin Utility Relocation	Summer 2021
Begin Construction	Summer 2023
Project Open to Traffic	2026

- Dominion Virginia Power – Elec.
- Cox Communications – Cable TV
- Verizon – Telephone & Fiber
- Washington Gas - Gas
- FCPW – Sanitary Sewer
- FCWA – Water Lines

= Milestones

Project Estimate and Funding

Cost Estimate* (\$ in Millions)	Programmed Funding	Proposed Sources	Funding Sources**
PE \$ 16	\$ 1.0		NVTA 70% (FY15/16)
RW/UT \$ 163.5	\$ 46.7		RSTP (Federal)
	\$ 10.0		Revenue Sharing
CN \$ 192.5	\$ 3.7		Local Funds (C&I)
Total \$ 372	\$ 127.0		NVTA 70% (FY18/23)
		\$ 183.6	SMART SCALE (Submitted Application 8/18) and/or Additional Sources: NVTA 70%, RSTP, Federal Grants, Developer Contrib., Local, and Other
	\$ 188.4	\$ 183.6	\$ 372

*Cost estimates subject to refinement as project develops.

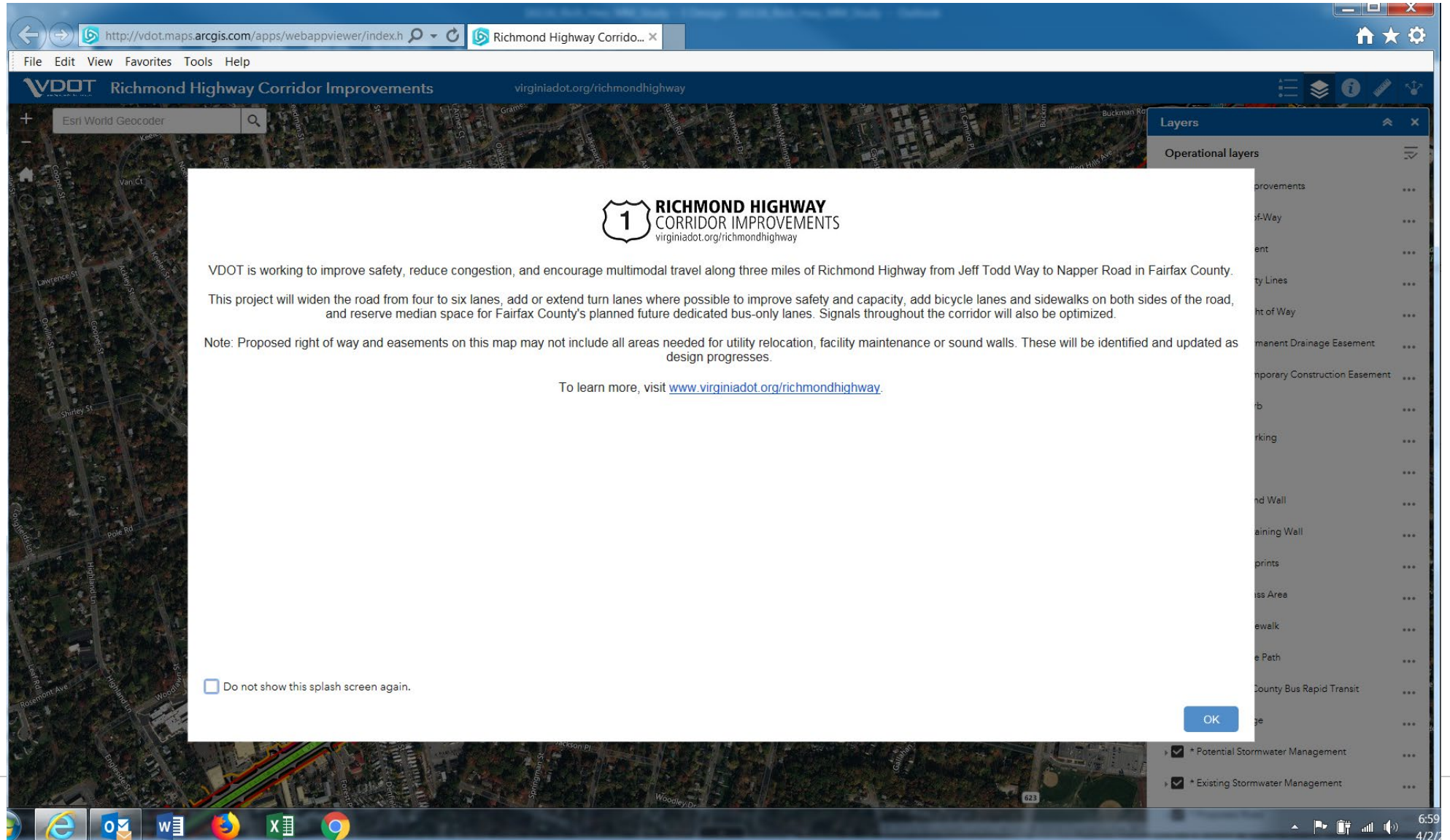
**Project is jointly funded with \$128 million from the Northern Virginia Transportation Authority (NVTA).

GIS Web Site Application

Available on the project webpage www.virginiadot.org/projects/northernvirginia/Richmond_highway.asp

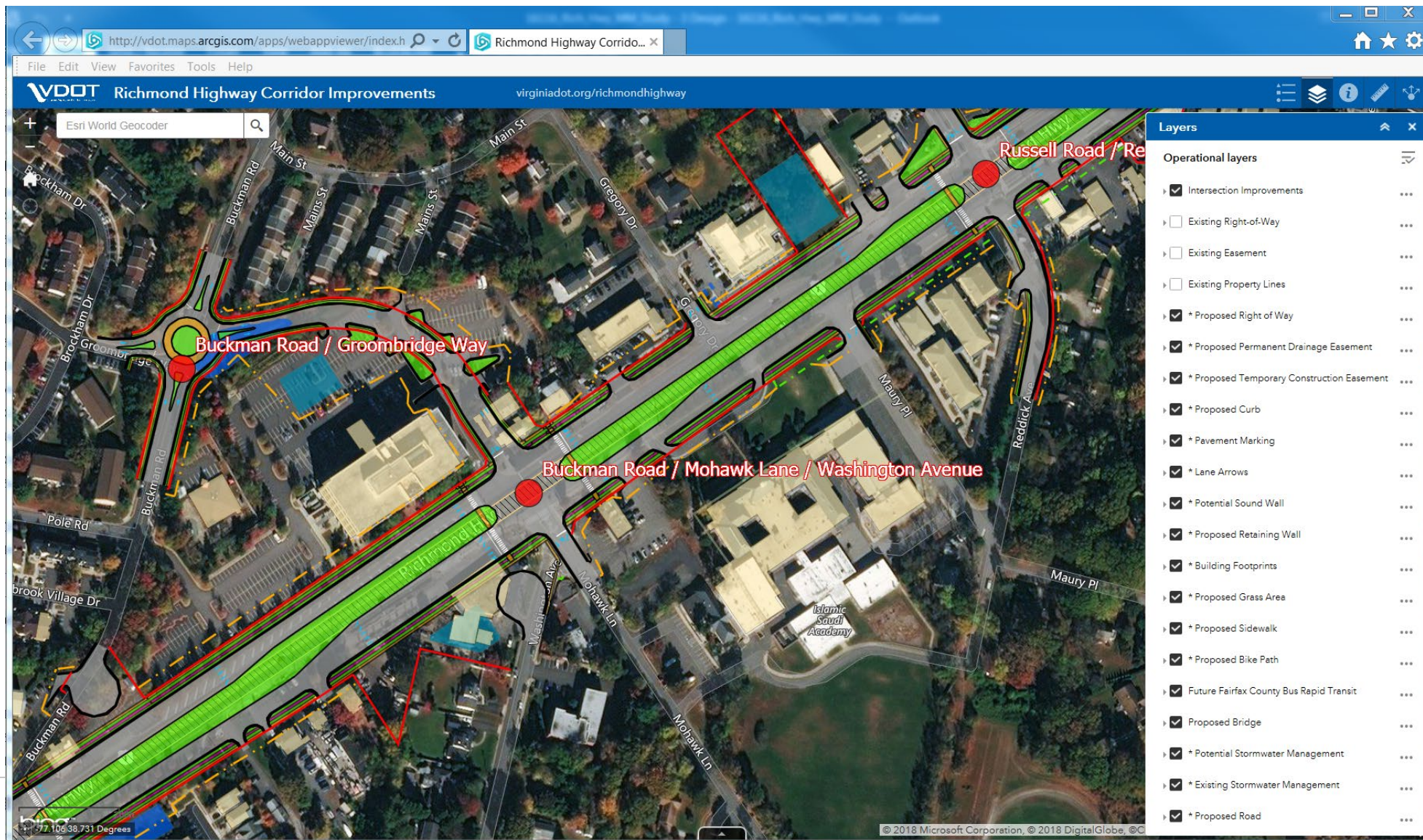
Opening page

Property impact information is preliminary in nature and subject to change as the design is refined.



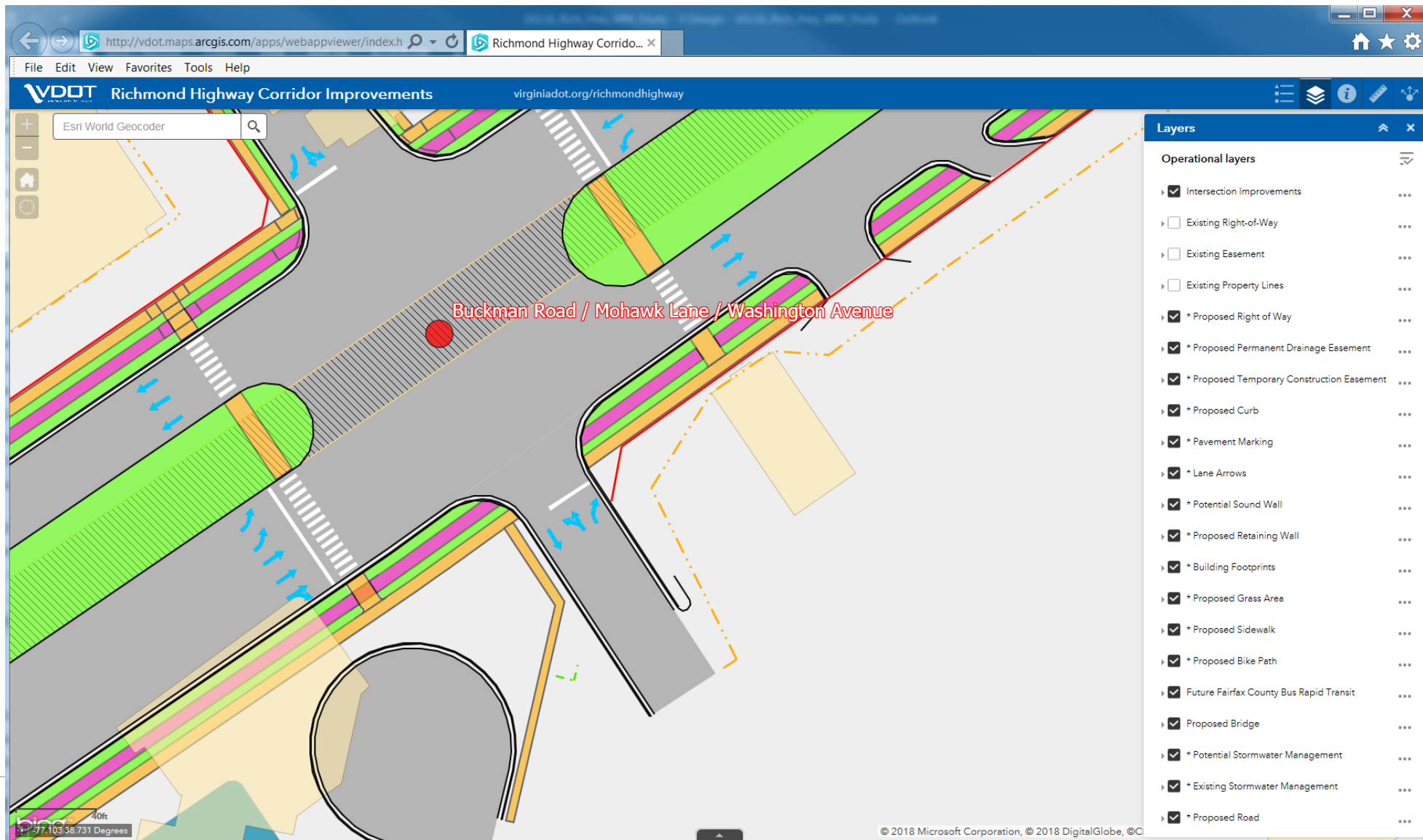
Intersection level view

Property impact information is preliminary in nature and subject to change as the design is refined.



Property view

Property impact information is preliminary in nature and subject to change as the design is refined.



How to Provide Comments

- **Mail:**

- Mail written comments to VDOT, attention Dan Reinhard, P.E.
VDOT Northern Virginia District, 4975 Alliance Drive, Fairfax, VA 22030

- **Email:**

- Email your comments or questions to RichmondHighway@vdot.Virginia.gov

**Please reference “Richmond Highway Corridor Improvements” in subject line
of all correspondence**



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Questions?

